



## APPENDIX G

### District of West Vancouver *Proposed* Development Permit No. 16-035

**CURRENT OWNER:** 0875410 B.C. Ltd.

**THIS DEVELOPMENT PERMIT APPLIES TO:**

**CIVIC ADDRESS:** 657 MARINE DRIVE

**LEGAL DESCRIPTION:** 014-020-840  
LOT 52, EXCEPT, FIRSTLY; PART IN REFERENCE PLAN  
2711, SECONDLY; PART ON HIGHWAY PLAN 30 DISTRICT  
LOT 1039 PLAN 2127  
(the 'LANDS')

**CIVIC ADDRESS:** 657 MARINE DRIVE

**LEGAL DESCRIPTION:** 014-020-858  
LOT 53, EXCEPT, FIRSTLY; PART IN REFERENCE PLAN  
2711, SECONDLY; PART ON HIGHWAY PLAN 30,  
DISTRICT LOT 1039 GROUP 1 NEW WESTMINSTER  
DISTRICT PLAN 2127  
(the 'LANDS')

**CIVIC ADDRESS:** 675 MARINE DRIVE

**LEGAL DESCRIPTION:** 009-125-680  
LOT A DISTRICT LOT 1039 PLAN 11209  
(the 'LANDS')

**CIVIC ADDRESS:** 660 CLYDE AVENUE

**LEGAL DESCRIPTION:** 014-020-785  
LOT 44 DISTRICT LOT 1039 PLAN 2127  
(the 'LANDS')

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**1.0 This Development Permit:**

- (a) imposes requirements and conditions for the development of the Lands, which are designated by the Marine Drive Local Area Plan as within the Park Royal - Clyde Avenue Sub-Area which is envisioned as a mixed-use, transit-oriented area at the gateway to the District of West Vancouver that allows for a mix of uses, including residential, mixed commercial residential buildings with street oriented commercial uses at grade, commercial office buildings, hotels and specialized residential facilities; and

- (b) is issued subject to the Owner's compliance with all of the Bylaws of the District applicable to the Lands, except as varied or supplemented by this Permit.

**2.0 The following requirements and conditions shall apply to the Lands:**

- 2.1 Building, structures, on-site parking, driveways, and site development shall take place in accordance with the attached **Schedule A**.
- 2.2 Zoning Bylaw No. 4662, 2010 is varied as follows:
  - a) Section 143.01(1)(a) Secure Bicycle Parking is varied in accordance with the attached **Schedule A**.
  - b) Section 143.02(1)(c)(ii) Short Term Bicycle Parking is varied in accordance with the attached **Schedule A**.
  - c) Section 143.02(3) Short Term Bicycle Parking is varied in accordance with the attached **Schedule A**.
- 2.3 Sprinklers must be installed in all areas as required under the Fire Protection and Emergency Response Bylaw No. 4366, 2004.
- 2.4 No wood burning fireplaces shall be installed, constructed or otherwise permitted on the Lands or in any building on the lands.
- 2.5 On-site landscaping shall be installed at the cost of the Owner in accordance with the attached **Schedule A**.
- 2.6 Sustainability measures and commitments shall take place in accordance with the attached **Schedule A**.
- 2.7 All balconies decks and patios are to remain fully open and unenclosed and the weather wall must remain intact.

**3.0 Prior to commencing site work or Building Permit issuance, whichever occurs first, the Owner must:**

- 3.1 Provide and implement a plan for traffic management during construction to the satisfaction of the District's Manager of Land Development.
- 3.2 Install tree, vegetation and/or hedge protection measures as required to the satisfaction of the District Arborist.
- 3.3 Submit a "Sediment and Erosion Plan" to the District's Environmental Protection Lead or Officer for approval, which the Owner shall comply with and be responsible for maintaining, repairing and implementing the sediment control measures.

**4.0 Prior to Building Permit issuance:**

- 4.1 Provide engineering civil drawings detailing works, including but not limited to:
  - (a) storm water management measures;
  - (b) site service connections;
  - (c) off-site servicing upgrades, including undergrounding of utility poles on the frontages of the Lands;
  - (d) new boulevard plan along the frontage of the site including curbs, separated bicycle lane, sidewalk and grading plan; and
  - (e) repaving along the frontage of the Lands,

which must be submitted for acceptance, and security provided for the due and property completion of the engineering works, all to the satisfaction of the District's Manager of Land Development.

- 4.2 Undertake design development of the public realm along the Marine Drive and Taylor Way road frontages that are in keeping with the Marine Drive / Taylor Way Gateway project as directed by Council, which must be submitted for acceptance to the satisfaction of the District's Director of Planning and Development Services;
- 4.3 Provide sufficient documentation to demonstrate compliance with Step 2 of the BC Energy Step Code and the use of a Low Carbon Energy System (LCES) which must be submitted for acceptance to the satisfaction of the District's Director of Planning and Development Services. For the purposes of this permit, a LCES means a mechanical system providing all thermal conditioning and all domestic hot water for heating for all new buildings primarily from low-carbon energy sources with the following characteristics:
  - (a) system seasonal average co-efficient of performance greater than two;
  - (b) modelled Greenhouse Gas Intensity of no more than 3 kg e/m<sup>2</sup>/yr; and
  - (c) any natural gas fired peak demand heating equipment is appropriately sized to augment the primary low carbon system under peak demand conditions.

## 5.0 Security for Landscaping

- 5.1 Prior to building permit issuance, security for the due and proper completion of the landscaping set forth in section 2.4 of this Development Permit (the "Landscaping Works") shall be provided in the amount of \$285,000 (the "Landscape Deposit") to the District in the form of cash or unconditional, irrevocable auto-renewing letter of credit issued by a Canadian chartered bank or credit union.
  - 5.2 Release of the Landscape Deposit:
    - (a) Following installation of the Landscaping Works and upon receipt of a certified letter or report by a Landscape Architect in good standing with the British Columbia Society of Landscape Architects to the District stating that:
      - a. the Landscaping Works have been installed substantially in accordance with **Schedule A**; and
      - b. any variations that may have been undertaken to the Landscaping Works are clearly identified, including but not limited to:
        - i. any adjustments to retaining walls,
        - ii. changes to the mixture or sizes of any plant materials or trees,
        - iii. completion of any off-site or boulevard works,
        - iv. any areas that received alternative treatment,
        - v. any paving changes, or
        - vi. any other additional or omitted plantings or alterations,
- together with a clear rationale and explanation thereof and stating

- c. that a final review with the landscape contractor or consultant of record has been completed, including provision of the date when this final review was completed on,
- d. whether there are any outstanding Landscape Works which are outstanding or which need attention, and
- e. notwithstanding outstanding works in 5.2(a)(d) above, that the Landscaping Works are complete,

then District will release 75% of the initial value of the Landscape Deposit. The remaining 25% of the initial value of the Landscape Deposit shall be retained by the District as a warranty deposit (the "Warranty Deposit") to ensure successful installation of the Landscaping Works.

- (b) After a one-year period following certification that the Landscaping Works have been completed, and upon final certification by a Landscape Architect in good standing with the British Columbia Society of Landscape Architects that the Landscaping Works are successful, the District will release the Warranty Deposit.

### 5.3 Additional Landscape Security

- (a) No occupancy shall be issued nor will any other final approvals be given for any individual building site identified as a "Parcel" on attached **Schedule A**, until:
  - a. all of the Landscaping Works are completed, or
  - b. the Owner provides security in addition to and separate from the Landscape Deposit, and in the amount of 110% of the value of the uncompleted Landscaping Works for that specific Parcel only (the "Additional Security Deposit") for the due and proper completion of the uncompleted or deficient Landscape Works for that specific Parcel only, as determined and certified by the consultant of record; and
  - c. the Additional Security Deposit will be released upon final certification by a Landscape Architect in good standing with the British Columbia Society of Landscape Architects following certification that all of the Landscaping Works on the Parcel have been completed.

5.4 In the event that the Landscaping Works are not completed as provided for in this Permit, the District may, at its option, enter upon, carry out and complete the Landscaping Works so as to satisfy the terms of the Development Permit, and recover the costs of doing so from the security deposited or recover any costs incurred over and above the amount of the security deposited, including the costs of administration and supervision.

6.0 This Development Permit lapses if the work authorized herein is not commenced within 24 months of the date this permit is issued.



THE COUNCIL OF WEST VANCOUVER APPROVED THIS PERMIT BY RESOLUTION  
PASSED ON \_\_\_\_\_.

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CORPORATE OFFICER

**FOR THE PURPOSES OF SECTION 6.0, THIS PERMIT IS ISSUED ON \_\_\_\_\_.**  
Council Report: August 9, 2021 (#3848658)

Schedule:

A – Architectural plans, landscaping, adaptability, and sustainability measures, etc.

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**SCHEDULE A**  
to DP 16-035



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ENERGY CODE LETTER

# EXECUTIVE PARC

MARINE + TAYLOR RESIDENTIAL  
657 MARINE DRIVE, WEST VANCOUVER

EXECUTIVE GROUP DEVELOPMENT ■ **dys** architecture

**EXECUTIVE PARC**  
**MARINE+TAYLOR RESIDENTIAL** 657 Marine Drive - West Vancouver BC

EXECUTIVE GROUP DEVELOPMENT ■ **dys** architecture



**RESPONSE TO MEMORANDUM**

No. A215294  
REVISED FOR MEMORANDUM - MARCH 28, 2019

**PROJECT TEAM**

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LEVEL	RENTAL	SALE	OTHER	TOTAL
1	5,080	5,509	4,292	14,881
2	5,080	7,504	1,500	14,514
3	6,094	7,900	1,422	15,416
4	-	13,995	1,422	15,416
5	-	13,256	1,422	14,678
6	-	12,128	1,399	13,527
7	-	9,236	954	10,190
8	-	6,615	976	7,592
ROOF	-	-	939	939
<b>TOTAL</b>	<b>16,253</b>	<b>76,574</b>	<b>14,325</b>	<b>107,153</b>

PLOT STAMP: 2019-Mar-28 @10:49am - P:\A215294 - Executive Group - Park Royal Site\CAD\SD\A215294\_Rezoning.dwg - A0.01

**PROJECT STATISTICS**

<b>657 MARINE DRIVE (TAYLOR WAY + MARINE DRIVE + CLYDE AVENUE)</b>			
<b>PROJECT STATISTICS</b>			
CIVIC ADDRESSES: 657 & 675 MARINE DRIVE AND 660 CLYDE AVENUE, WEST VANCOUVER, BC			
EXISTING ZONING		C2	
LOT SIZE	657 MARINE DRIVE	18,633 SF	C2
	675 MARINE DRIVE	24,324 SF	CR2
	660 CLYDE AVENUE	10,908 SF	CR1
	<b>TOTAL</b>	<b>53,865 SF</b>	
<b>LEGAL DESCRIPTION</b>			
LOT A PLAN 11209, LOTS 44, 52, AND 53, DISTRICT LOT 1039 NWD			
LOT 53, EXCEPT FIRSTLY PART IN REFERENCE PLAN 2711, SECONDLY PART ON HIGHWAY PLAN 30 GP 1 PLAN 2127			
LOT 52, EXCEPT FIRSTLY PART IN REFERENCE PLAN 2711, SECONDLY PART ON HIGHWAY PLAN 30 PLAN 2127			
<b>SITE COVERAGE</b>		<b>BUILDING FOOTPRINT</b>	<b>PERCENTAGE</b>
657 MARINE DRIVE (RESIDENTIAL BUILDING)		19,788 SF	37%
660 CLYDE AVENUE (COMMERCIAL HERITAGE)		3,156 SF	6%
<b>TOTAL SITE COVERAGE</b>		<b>22,944 SF</b>	<b>43%</b>
<b>BUILDING HEIGHT PROPOSED</b>			
<b>RESIDENTIAL</b>		94'-8" (28.9 M)	
<b>COMMERCIAL/HERITAGE</b>		13'-6" (4.1 M)	
<b>SETBACKS PROPOSED</b>			
<b>RESIDENTIAL</b>			
FRONT YARD (8TH STREET)	39'-2" - 43'-7" (10.9 - 13.3M)		
EXTERIOR SIDE YARD (MARINE DRIVE)	22'-6" - 41'-0" (6.9 - 12.5M)		
EXTERIOR SIDE YARD (TAYLOR WAY)	47'-4" - 77'-10" (14.4 - 23.7M)		
INTERIOR SIDE YARD (PARKADE SOUTH WALL)	16'-1" - 20'-6" (4.9 - 6.3 M)		
INTERIOR SIDE YARD (PARKADE EAST WALL)	12'-4" - 29'-8" (3.8 - 9.0M)		
INTERIOR SIDE YARD (EAST)	10'-7" - 33'-9" (3.2 - 10.1M)		
<b>COMMERCIAL/HERITAGE</b>			
FRONT YARD (CLYDE AVENUE)	15'-4" (4.7 M)		
SIDE YARD (EAST)	2'-0" (0.6 M)		
SIDE YARD (WEST)	10'-4" (3.1 M)		
<b>NUMBER OF UNITS PROPOSED</b>			
CONDOMINIUM RESIDENTIAL (FOR SALE)		68	
RENTAL RESIDENTIAL		21	
<b>TOTAL</b>		<b>89 UNITS</b>	
<b>FLOOR AREA RATIO (INCLUDING 660 CLYDE HERITAGE BUILDING)</b>			
PER CLYDE AVENUE PLAN		1.75 FAR	
BONUS FOR HERITAGE AND RENTAL HOUSING		0.25 FAR	
<b>TOTAL ALLOWED</b>		<b>2.00 FAR</b>	
<b>PROPOSED</b>		<b>1.99 FAR</b>	
<b>FLOOR AREA SUMMARY</b>			
<b>LEVEL NO.</b>	<b>TOTAL (SF)</b>	<b>UNITS (TOTAL)</b>	<b>RENTAL ADAPT.</b>
ROOF & MECH	939	-	-
89PH	7,592	3	-
7	10,190	6	- 1
6	13,527	12	- 3
5	14,678	13	- 3
4	15,416	14	- 3
3	15,416	14	7 3
2	14,514	13	7 3
1	14,881	14	7 2
P1	-	-	-
P2	-	-	-
HERITAGE BUILDING	3,156	-	-
<b>SUBTOTAL</b>	<b>110,309</b>	<b>89</b>	<b>21 18</b>
<b>EXCLUSION</b>		<b>PERCENTAGE</b>	<b>24% 20%</b>
AMENITY L1	1,653	-	-
ENTRANCE LOBBY	1,239	-	-
<b>TOTAL</b>	<b>107,416</b>		
<b>PARKING PROPOSED</b>			
<b>RESIDENTIAL</b>		<b>ACTUAL STALLS</b>	
REGULAR SIZE		73	
SMALL CAR		20	
ACCESSIBLE		2	
<b>REQUIRED PARKING SPACES 76-125</b>		<b>95 PER UNIT 1.07</b>	
<b>RESIDENTIAL VISITOR</b>		REGULAR SIZE	
SMALL CAR		5	
ACCESSIBLE		3	
<b>1 COMMERCIAL STALL REQUIRED FOR EVERY 37.2 SQM</b>		<b>1 13%</b>	
<b>TOTAL REQUIRED</b>		<b>8</b>	
<b>TOTAL</b>		<b>112 SPACES</b>	
<b>BIKE STORAGE LOCKERS</b>		<b>TOTAL 93 PER UNIT 1.04</b>	

**CLIENT**



**EXECUTIVE GROUP DEVELOPMENT**

NO. | DATE | ISSUE  
01 | 2018.05.17 | ISSUED FOR DRG  
02 | 2018.11.19 | ISSUED FOR PCM  
03 | 2019.03.28 | RESPONSE TO MEMORANDUM

NO. | DATE | REVISION

**PROJECT**

**EXECUTIVE TAYLOR MARINE + TAYLOR RESIDENTIAL**

657 Marine Drive  
West Vancouver BC

**PROJECT DETAILS**

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Written dimensions shall have precedence over related dimensions. Contractors shall verify and be responsible for all dimensions and conditions on the job and dys architecture shall be informed of any variations from the dimensions and conditions shown on the drawings. These drawings shall be submitted to dys architecture for review before proceeding with construction.

PROJECT A215294  
DRAWN JL CHECKED NC

SCALE N.T.S.  
DATE NOV. 19, 2018

**A0.01**



## EXECUTIVE SUMMARY

### 01 Executive Summary

The proposal put forth in this application for Rezoning and Development Permit consists of a consolidation of four parcels (3 civic addresses) into one site for redevelopment. The development will consist of one multi-storey building comprised of a mid-rise volume and a low-rise podium structure over two levels of underground parking, a restored heritage office building and a public parkette dedication. The building program is primarily market residential for sale and dedicated residential units for rental purposes.

The site is located at a prominent intersection in the District of West Vancouver. With the site's prominent location, the new building at the northeast corner of Marine Drive and Taylor Way is positioned as a "Gateway" feature into and out of the District of West Vancouver. It will reinforce the Park Royal intersection at Marine and Taylor and further enhance the Park Royal shopping district and the Clyde Avenue West of Taylor neighbourhood.

Outlined in greater detail within the Rezoning and Design Rationale the development program seeks to repurpose the site to meet the objectives of the Official Community Plan for this neighbourhood. The proposal is comprised of the following key aspects:

- **Heritage**

The redevelopment will consist of the rehabilitation of a primary building from the municipal's west coast survey of significant architecture 1945-1975. Currently on the site at 660 Clyde Avenue is the Woyat-Bowie office building designed by Fred Hollingsworth and Barry Downs. The building will be restored as per an approved conservation plan.

- **Residential**

The program for the redevelopment will increase the housing stock with secured market rental and for sale residential units. The building will bring new residents into the neighbourhood with 89 dwelling units and a varying mix of housing types and sizes ranging from one-bedroom units to larger family-oriented suites. Of the 89 residential units proposed with this development, there are 21 units dedicated for rental purposes (approximately 24%).

- **Public Parkette**

Provision of a new green space with a public parkette at the intersection of Marine Drive and Taylor Road is proposed. The public parkette will complement the redevelopment by bringing landscape into an urban setting. Elements and patterns are inspired by the nearby Capilano River, which carry forth into the urban environment setting to create a stage for relaxation and a transition between the urban fabric and West Vancouver's natural surroundings.

Overall, the proposed development will bring a balanced architectural language that combines sensitivity to the historical and natural features of the neighbourhood with a modern vocabulary inspired by the context of the surrounding built environment and the west coast aesthetics. The design approach was to be thoughtful and to be contextually aware of the rich features of the area to inform the overall form and character of the building.



Aerial View Looking North East

CLIENT



**EXECUTIVE**  
GROUP DEVELOPMENT

NO. | DATE | ISSUE  
01 | 2018.05.17 | ISSUED FOR DPC  
02 | 2018.11.19 | ISSUED FOR PCM  
03 | 2019.03.28 | RESPONSE TO MEMORANDUM

NO. | DATE | REVISION

PROJECT

**EXECUTIVE PARC  
MARINE + TAYLOR RESIDENTIAL**

657 Marine Drive  
West Vancouver BC

**EXECUTIVE SUMMARY**

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Written dimensions shall have precedence over related dimensions. Contractors shall verify and be responsible for all dimensions and conditions on the job and dys architecture shall be informed of any variations from the dimensions and conditions shown on the drawings. Shop drawings shall be submitted to dys architecture for review before proceeding with fabrication.

PROJECT A215294  
DRAWN JL CHECKED NC

SCALE N.T.S.  
DATE NOV. 19, 2018

**A0.02**

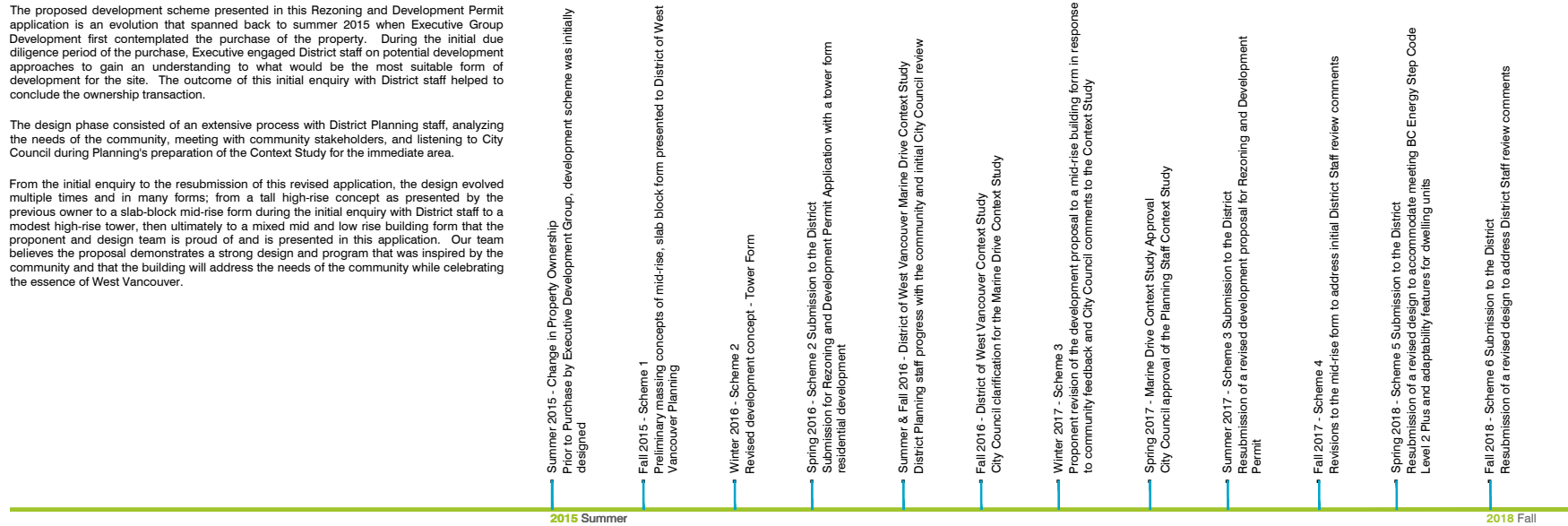
# PROJECT HISTORY

## 02 Project Evolution and Timeline

The proposed development scheme presented in this Rezoning and Development Permit application is an evolution that spanned back to summer 2015 when Executive Group Development first contemplated the purchase of the property. During the initial due diligence period of the purchase, Executive engaged District staff on potential development approaches to gain an understanding to what would be the most suitable form of development for the site. The outcome of this initial enquiry with District staff helped to conclude the ownership transaction.

The design phase consisted of an extensive process with District Planning staff, analyzing the needs of the community, meeting with community stakeholders, and listening to City Council during Planning's preparation of the Context Study for the immediate area.

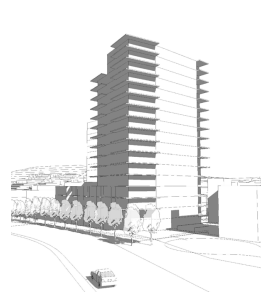
From the initial enquiry to the resubmission of this revised application, the design evolved multiple times and in many forms; from a tall high-rise concept as presented by the previous owner to a slab-block mid-rise form during the initial enquiry with District staff to a modest high-rise tower, then ultimately to a mixed mid and low rise building form that the proponent and design team is proud of and is presented in this application. Our team believes the proposal demonstrates a strong design and program that was inspired by the community and that the building will address the needs of the community while celebrating the essence of West Vancouver.



2015 SUMMER



2016 WINTER



2016 SPRING



2016 SUMMER



2017 SUMMER



2018 FALL

NO.	DATE	ISSUE
01	2018.05.17	ISSUED FOR DRC
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03	2019.03.28	RESPONSE TO MEMORANDUM

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**PROJECT**  
**EXECUTIVE PARC MARINE+TAYLOR RESIDENTIAL**

657 Marine Drive  
West Vancouver BC

**PROJECT HISTORY**

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## REZONING RATIONALE

### 01 Site Description

The development site is located adjacent to Park Royal under the Commercial Development Permit Area designation and Marine Drive Local Area Plan. The project consists of four separate parcels to be consolidated and rezoned for redevelopment as contemplated in the current Official Community Plan. These parcels include: 660 Clyde Avenue, 657 Marine Drive and 675 Marine Drive. The consolidation of these four parcels to create a J-shaped lot that partially fronts three streets; Clyde Avenue to the north, 6th Street to the east albeit a cul-de-sac, Marine Drive to the south, and Taylor Way to the west.

The site area measures approximately 53,865 sf (5,004 sm) and consists of varying dimensions due to the unique shape of the site. The site has a gentle slope between 3% and 5% from Clyde Avenue up to Marine Drive, but is generally flat throughout with a local grade change at the southeast corner of the site that transitions up to the Marine Drive roadway and overpass.

### 02 Context Plan

#### Neighbourhood Overview

The project is in a unique location in the community as it serves as the principal entry point into the District of West Vancouver. It is in close proximity to a number of amenities, including the Capilano River, Park Royal Shopping Centre, public transit, and the Lions Gate Bridge connecting to Downtown Vancouver.

Within the Clyde Avenue west of Taylor neighbourhood, there is a mix of residential and commercial uses and building types. The immediate context consists of various conditions and structures that influence the edges and interface. On the northwest edge of the site, on the east side of Taylor Way, the site is predominately screened by a three-storey above grade parking structure that serves the Park Royal Shopping Centre. Low-rise commercial office buildings are located on the two parcels to the immediate northeast corner of the site, while a four storey rental apartment building is sited immediately to the east. Across Clyde Street to the north, there is a four-storey senior independent living building. Across Marine Drive, on the south side are two high-rise residential towers known as the West Royal Towers. Further east of the site next to the Capilano River is the distinctive Water's Edge residential development.

### 03 Applicable Zoning, Policies, and Guidelines

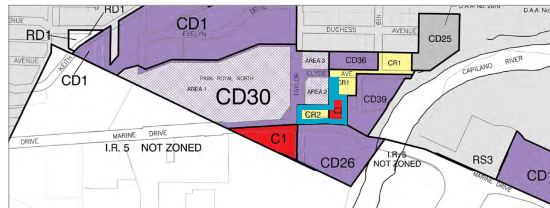
Existing Zoning: CR1 + CR2 (Commercial Restricted) & C2 (Commercial)  
 Proposed Zoning: CD (Comprehensive Development District)

#### Guidelines & Policies:

Marine Drive Local Area Plan and Guidelines - April 2017



Consolidated Parcels



Zoning Map

<b>Existing site</b>		Commercial	<b>C</b>	
One-Family Dwelling	<b>RS</b>	Commercial Restricted	<b>CR</b>	
<b>Proposed</b>	<b>CD</b>			



Amenities Aerial View of the Heart of the District of West Vancouver



Main Arteries Intersecting Site Aerial View of West Vancouver and Downtown Vancouver

#### PROJECT

### EXECUTIVE PARC MARINE+TAYLOR RESIDENTIAL

657 Marine Drive  
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## REZONING RATIONALE continued

### 04 Rezoning Rationale

The purpose of this Rezoning application is to consolidate the four separate parcels for redevelopment as a singular site. The civic addresses related to these parcels are: 660 Clyde Avenue, 657 Marine Drive and 675 Marine Drive. Each existing parcel is currently zoned for commercial use.

This rezoning application is guided by the existing policies for this area referenced as the Marine Drive Local Area Plan and the Park Royal, and Clyde Avenue Sub-Area, and Guidelines.

Rezoning is sought to allow for residential uses and to include dwelling units for market sale and market rental in a multi-storey building. A significant portion of the residential building will be dedicated to market residential rental units of varying sizes which will promote greater housing options for people living in the District. The residential component will include common building amenity spaces and underground parking.

In addition to the residential component, the rezoning seeks to refurbish the existing historical commercial structure on the 660 Clyde Avenue and designate it as a heritage building. In addition to the extensive landscape proposed in the development, the rezoning of the site also proposes to include new open green space dedicated for public use at the corner of Marine Drive and Taylor Way intersection. This creates a park-like transitional zone between the river and the urban activities along Marine Drive.

Attention was placed on the development edges to ensure the public realm and the relationships are strengthened. The Marine Drive street edge strives to promote a friendly pedestrian streetscape. Great care was taken to integrate slopes and steps for universal access and to create a sense of entry and privacy for residents and visitors.

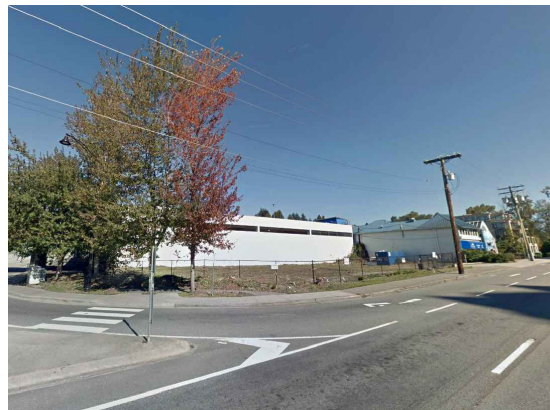
The building will become an important entrance feature to the District of West Vancouver. It seeks to meet and exceed the District's requirement with rental housing, heritage restoration and public green space. The design proposes a high-quality residential building in an urban landscape setting that further enhances this part of Clyde Avenue and serve as a positive welcome to West Vancouver experience for residents and visitors to the District.



660 Clyde Avenue Front Entrance



660 Clyde Avenue View from Adjacent Parking Lot



Marine Drive at Taylor Way Current Site



Marine Drive at Taylor Way Proposed Site

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**EXECUTIVE PARC  
MARINE+TAYLOR RESIDENTIAL**

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## REZONING RATIONALE continued

### 05 Response to the Marine Drive Local Area Plan and Guidelines

In reference to the Marine Drive Local Area Plan and Guidelines, and the Park Royal - Clyde Avenue Sub-Area, the proposed development meets or exceeds the planning objectives and land use concept.

This includes:

- Reinforce the town centre around Taylor Way by the Park Royal Shopping Centre;
- Connection between the two town centres at Taylor Way / Clyde Avenue and Lions Gate / Klahanie;
- Engages the natural environment and reinforces the relationship with the Capilano River;
- Views and legibility are enhanced from the Lions Gate Bridge;
- Respond to the context with sensitive design influenced by the neighbouring Park Royal Shopping Centre, West Royal Towers, built form expression, materials, scale and composition, proportion, public space organization and public realm treatment;
- Connect to Park Royal by enhancing and extending pedestrian movement along Marine Drive between the shopping centre and residential uses to the east;
- Meet housing objectives with increased density to provide rental housing where walking, cycling or taking transit can lower transportation cost for residents;
- Secure appropriate amenities with improved public realm features along Marine Drive, connection to the existing natural asset such as the Capilano River, and new open space at the north east corner Marine Drive and Taylor Way;
- Minimize trip generation with residents living in close proximity to employment, shops and services by foot or by bus, reducing pressure off the road system.

The proposed built form is consistent with the design guidelines:

- Urban residential with engaged frontage along Marine Drive and public space at corner;
- Reinforces Planning objective of a primary cluster of taller buildings defining heart of centre with street-fronting lower scale;
- Provision of public spaces;
- Complement existing buildings with variation of the building height to create a dynamic and interesting skyline;
- Mid-rise building to transition between existing low and high-rise forms, creating open space at grade and responding on character to surrounding uses and natural features
- Building height of 6 and 8 storeys to respond to context;
- Building siting and height respond to and frame significant public views looking down Taylor Way and along Marine Drive;
- Provide new urban space with a public open space oriented towards the intersection of Taylor Way and Marine Drive;
- Screening of the existing parkade;
- A notable and enhanced public walkway adjacent Marine Drive that connects the new public open space with the Capilano River, drawing the character of the river environment;
- Potential public art opportunities at various locations that can reflect its importance and contribute to the sense of place.

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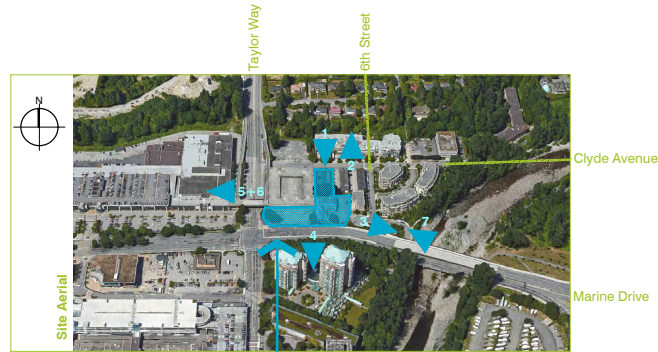
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PROJECT Site



No.1 Clyde Avenue Looking North



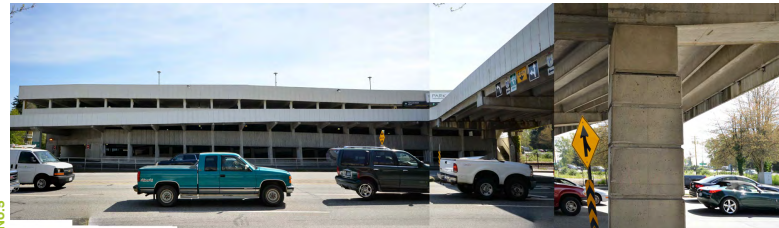
No.2 Clyde Avenue Looking South



No.3 6th Street Looking West



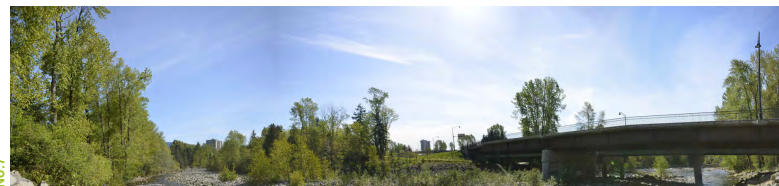
No.4 Marine Drive Looking North



No.5 Taylor Way Looking East



No.6 Taylor Way Looking East



No.7 6th Street / Capilano River Looking East

PROJECT

**EXECUTIVE PARC MARINE+TAYLOR RESIDENTIAL**

657 Marine Drive West Vancouver BC

**URBAN CONTEXT**

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URBAN CONTEXT continued

02 Shadow Studies

March 20 Spring Equinox + June 21st Summer Solstice



10:00 AM March 20th Spring Equinox



12:00 PM March 20th Spring Equinox



2:00 PM March 20th Spring Equinox



10:00 AM June 21st Summer Solstice



12:00 PM June 21st Summer Solstice



2:00 PM June 21st Summer Solstice

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**dys** architecture  
260 - 1770 Burrard Street Vancouver BC V6J 3G7  
tel 604.669.7710 www.dysarchitecture.com

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URBAN CONTEXT continued

02 Shadow Studies

Sept 22nd Fall Equinox + Dec 21st Winter Solstice



10:00 AM Sept 22nd Fall Equinox



12:00 PM Sept 22nd Fall Equinox



2:00 PM Sept 22nd Fall Equinox



10:00 AM Dec 21st Winter Solstice



12:00 PM Dec 21st Winter Solstice



2:00 PM Dec 21st Winter Solstice

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URBAN CONTEXT continued

03 Urban Context Aerial Views



Aerial View Looking North West



Aerial View Looking North East



Aerial View Looking South East



Aerial View Looking South West

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## DESIGN RATIONALE

### 01 Introduction

The project site is next to the key intersection of the Lions Gate Bridge and Marine Drive. Moving west from this intersection over the Capilano River the site location is at the entry point to the District of West Vancouver. Its prominent location addressing all of the traffic moving into and out of West Vancouver creates the opportunity for a 'Gateway' building which can signify entrance.

### 02 Existing Context Integration

The District of West Vancouver provides a rich and varying context. Being part of the 'North Shore' it is in close proximity to nature and famously has many built examples of West Coast Modern architecture. The project site benefits from these site factors with views of mountains to the North, Capilano River to the East, Stanley Park and the Vancouver skyline to the South, and English Bay to the West.

At the northern portion of the site sits a prime example of West Coast Modern commercial architecture. This unique small office building designed by Fred Hollingsworth and Barry Downs is characterized by a striking roof line and a wood and glass entrance which are used to link together two white stucco volumes. It is the intention of this application to retain this building as it is not only historically significant but also adds to the character of the proposed development.

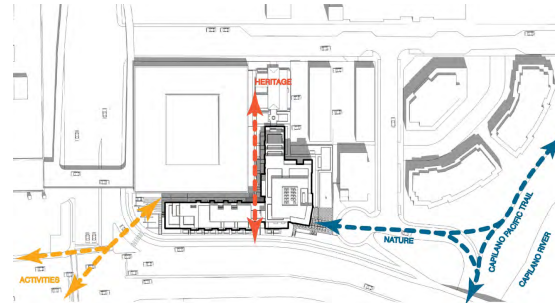
On the south side of the Marine Drive are the West Royal towers, two 25 storey apartment towers whose design capitalizes on the available views through the height of the buildings and from generous balconies. These towers express a port-modern architectural style predominately of concrete and glass. Since its completion in the early 1990's, these buildings have inherited the role of "Gateway" to West Vancouver.



660 Clyde Avenue - Dental Clinic View Looking North



Park Royal Mall View Looking South East



To the west of the West Royal towers and the project site, is the Park Royal Shopping Centre, a large shopping mall on two parcels which has been recently redeveloped with expanded commercial retail spaces and structures. The commercial complex has future plans for new residential buildings and further expansion of its retail base. The general architectural style of the renewed Park Royal Shopping Centre is of a modern, urban aesthetics and form.

The residential communities to the east and north of the project site bring a unique architectural style to the edge conditions of the project site. The Water's Edge residential complex incorporates a blend of Georgian architecture with west coast detailing by adopting a strong, robust form of stone and solid materials with refined elements. The crescent roadway through the site is flanked with extensive landscaping, solid planters and villas that bring a refined street appeal. The Amica Seniors Independent Living facility designed by dys architecture (formerly Davidson Yuen Simpson Architects) back in 2005 highlights a simple contemporary west coast architectural language with the use of extensive brick masonry, concrete walls with large windows, extensive balconies and landscaping.

The design response to the site respects and celebrates the community. All of the contextual factors noted above guided the overall design approach of the proposed building by adopting the mixed architectural vocabulary found in the surrounding context and blending the west coast style of West Vancouver into the design.



Capilano River View Looking North

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## DESIGN RATIONALE continued

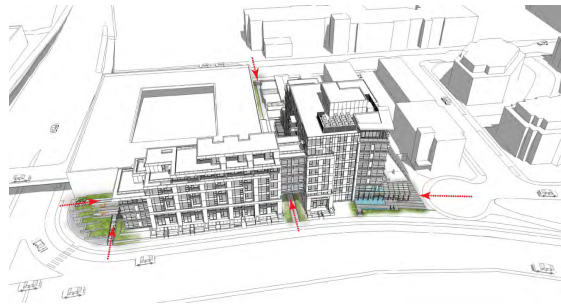
### 03 Site Rationale

Marine Drive is the main thoroughfare through West Vancouver, the low-rise podium and main entrance to the main mid-rise building form create a strong frontage to this. The low-rise podium is set back to allow for a future bike path and boulevard along Marine Drive, while incorporating a street wall condition to reinforce the public realm. The West portion of the site next to the intersection of Taylor Way and Marine Drive is to be dedicated to the district as a new public parkette and open space. The main mid-rise building form is set back 37 ft. from the east and 30 ft. from the west property lines with the expectation that the adjacent sites will develop as mid-rise residential in the future.

Between the proposed residential building and the retained heritage building is an open outdoor space for use by the residents and commercial occupants. A landscaped area above the parking ramp will serve as a children play area. An aperture on the lower level at the low-rise and mid-rise junction serves as a visual and physical connector. The aperture leads pedestrians through an extensively landscaped "mews" which serves the number of fronting ground-oriented units.

Pedestrian access is either from Clyde Avenue, 6th Street and Marine Drive. The existing opening between the heritage office building and the parkade will be retained but enhanced with landscape, allowing occupants to enter the site from the north. The aperture off Marine Drive is an architectural feature strategically designed into the building and planned for the ground to introduce access for residents and to visually connect the public with the new Mews and more importantly to highlight the heritage building at the end of the Mews. Main access to the building will be from the cul-de-sac off 6th Street. The entrance is expressed with a west coast and heritage influence to the entry canopy and entry doors aligned to the cul-de-sac. Firefighting will be designated from this side of the site.

Commercial, residential, and visitor vehicle access is from the existing cul de sac on 6th Street which leads to the parkade ramp planned to be parallel with the neighbouring apartment parking ramp.



#### Pedestrian Continuity

Multiple access points into the site to provide various types of experiences for pedestrians and visitors - To provide visual and spatial openness to celebrate and connect with history and community.

### 04 Use, Density and Parking

The proposal contains 89 residential units plus 1,676 sf. of amenity area with a proposed floor area ratio of 2.0. Of the 89 residential units, there are 21 designated units for rental purposes with the remaining for market sale. Parking is limited to 2 levels below grade as the water table is at an approximate geodetic elevation of 2.9 m. Provided parking is 113 spaces with 92 bike and storage spaces.

### 05 Form and Height

The proposed form and height promote the strategy of increasing density sensibly with a residential building which respects its architectural context, makes the most of the surrounding views and with a restricted footprint, will lessen the impact on views of the neighbouring properties. It is part of a new phase of multi-family residential for the North Shore following examples set by the recent Ambleside and Evelyn developments.

The proposed development features a terraced mid-rise building typology that span the unique shape of the site. The building is to respond sensitively to concerns of excessive height, obstruction of views for the surrounding neighbourhood, and overall size of a building form.

The main volume which consists of a taller mid-rise component is generally eight storeys in height but the top level expands vertically to encapsulate and blends with the rooftop mechanical room on the top of the building. Extending vertically help screen the required rooftop mechanical space, while providing living space to optimize the views and give access to an extensive roof surface for private outdoor space to the upper floor residents.

The low-rise form creating the podium base that extend west and north serves to create an appropriate scale to the public realm and relate to the immediate context. The podium facing Marine Drive is generally six storeys in height with localized pop-ups above level six for access to roof top terraces and living space to optimize views and access to daylight.



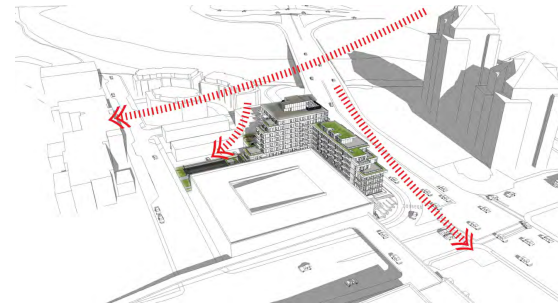
#### Context

Architectural form and character derived and influenced by the neighbouring structures and the natural setting. Solid base with street wall podium grounds the building while ascending vertically with transparency and horizontal layering and patterning.

This podium element along Marine Drive transitions from six storeys down to four storeys where it faces the new open green space at the intersection.

The podium element that extend north from the mid-rise volume transitions to a two storey townhouse component at grade, then ultimately to a single storey elevator and stair element facing the single-storey heritage building.

The overall form of the proposed building which blends a low-rise and mid-rise element is sited opposite the West Royal towers which will then formally frame a 'Gateway' to West Vancouver. The building proposes an overall height of 99 ft. (30 m) measured from grade.



#### Massing & Hierarchy

Influenced by the nearby mountainous terrace, natural surrounding, and existing built environment, the massing strategy is to respect the context, transitioning from the taller form to the adjacent heritage structure and public green space. Optimize roof terraces with landscape and outdoor living.





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## DESIGN RATIONALE continued

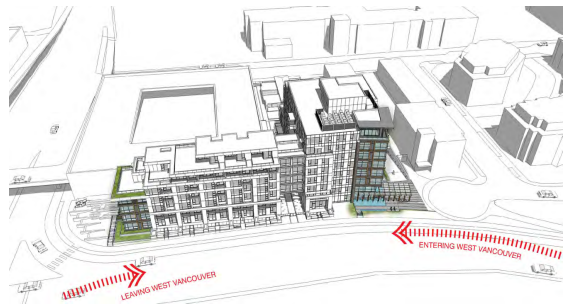
### 06 Building Articulation and Architectural Expression

The core guiding principle for the design is to create a building that is contextually sensitive to the immediate surrounding area and represents a high-quality architectural design that is greatly prevalent throughout the District of West Vancouver, while celebrating the west coast modern aesthetics and design sensibility.

The design represents a formal language that symbolize a solid, robust building form with a podium level and lower street wall inspired by the nearby residential buildings like Water's Edge and Amica Seniors, while the main volume "body" of the building is expressive in a horizontal vocabulary that follows the low profile natural terrain. The canopy expressions, brick masonry and paving, window colours and patterns capture the essence of a west coast style. Architectural elements and details celebrate the heritage building as displayed at the building's main entry vestibule, upper roof canopy, and building form. The main entrance is expressed as a timber clad entryway which is conceived as a modern interpretation of the care and craftsmanship that went into of the existing 660 Clyde building.

The south east corner of the proposal is the main expression of the mid-rise volume. A glass façade to the south and east, incorporating an expressive canopy structure of heavy-timber, wood, and steel at the top of the building and at grade, provides the key statement that the building is part of the 'Gateway' to West Vancouver. The vertical character of this element anchors the prominent corner, with dark frames and bold horizontal window mullions patterns reminiscent of the typical low-profile west coast building expression and natural setting. The horizontal emphasis interprets a west coast modern architecture, while providing inhabitants wide views of the surrounding natural context. This is further emphasized when the building terraces at the upper levels.

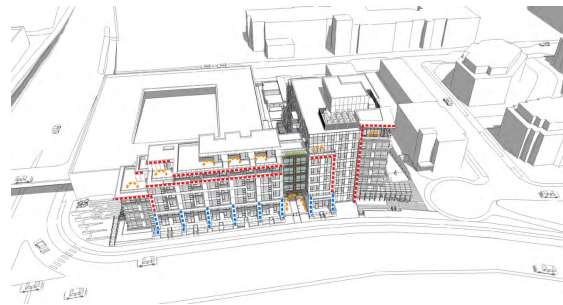
Dually significant is the south west corner of the site at the intersection of Marine Drive and Taylor Way. Emphasis to this secondary highly-visible corner is achieved with a terracing building form and a horizontal expression. The podium is conceived as a 'building in the landscape' which terraces to relate to the massing and context. A public open space is located to compliment the building aesthetics while providing the community a space for gathering and relaxation. The open space creates a gentle pause to the urban and vehicular setting along this busy intersection.



**Express the Gateway**  
 Strengthen the prominent corners of the building with simple patterns and architectural expression.

Connecting the two most visual corners of the building are residential units at grade facing the street which incorporate an appropriately scaled brick masonry street wall framing a porch-like landscaped element that interfaces with the street, an aperture that connects to an inner pedestrian mews and to the heritage building. The street edge with the extension of patios spaces from the ground floor units help engage the public realm, creating experiences for pedestrians and visitors during their journey from the urban setting to the river's edge to the east. With a double height lobby area fronting 6th Street to the east, it visually connects to Marine Drive to add interest and reinforce a strong interaction between building and street.

The internal north and west facing side of the proposed building interfaces with a significant parkade structure. The north side of the podium features open walkways to provide improve livability to those units with opportunities for additional windows to allow greater access to views and natural ventilation, additional "eyes on the street" to the mews and pathways, and future proofing to adapt to the redevelopment of the parkade site and possibly an expanded green space. Rather than a solid concrete exterior wall, an open walkway featuring architectural and landscape elements create interest from views up the Taylor Way hillside.



**Scale**  
 A low building form with terracing and repetitive architectural elements help strengthen a human scale and relationship with the community.

### 07 Adaptable Design

This proposed project will provide 18 residential units (20% of total) that will meet Level 2 of the City of North Vancouver's Adaptable Design Guidelines which is being used for reference for this application. In addition, the remaining residential units in the project will feature basic adaptable design elements to allow for future needs. These basic adaptable features include:

- In one bathroom, solid blocking provided in walls of tub / shower and toilet areas and behind towel bars;
- Pressure balanced tub / shower valves;
- One bedroom is have a clear opening door width of not less than 800 mm (31.5");
- Bedrooms to have a telephone jack;
- Windows to have easily grasped and operated mechanism for opening and locking;
- Kitchens to have lever handle faucets and cabinet handles which can be easily used with an open hand (for example "D" or "J" cabinet handles);
- Kitchens designed so that the range and sink are adjacent or with a continuous counter in between;
- Adjustable shelves in cabinets;
- Outlets, switches and controls to be located between 455 mm (18") and 1,200 mm (48") above the floor;
- Non-slip flooring in the kitchen and one bathroom;
- Unit entries to have door handles at 1,000 mm (40") above the floor with deadbolts placed immediately above or below.



**Edges**  
 Relationship between indoor and outdoor - Ground-oriented units to soften adjacent site conditions, strengthen the public realm and relationship to the community - Forward planning to adapt to future conditions with redevelopment of adjacent sites.

PROJECT

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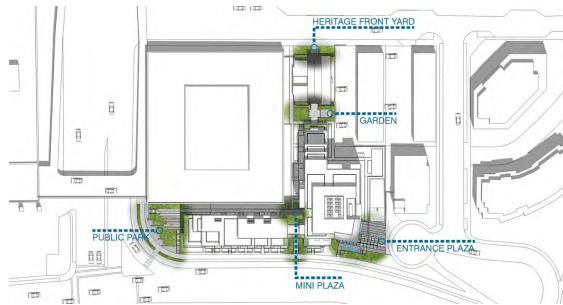
## DESIGN RATIONALE continued

### 08 Public Realm and Amenity

The public realm will be enhanced significantly with the addition of the public open space at the western end of the development, ground-oriented units with extensive patio space facing Marine Drive and into the Mews, an architectural aperture to highlight the internal pedestrian mews and the heritage building in the distance, resident indoor amenity space facing the aperture and street, terracing from slope street to the entry level with landscape and water feature, a double height lobby volume, and an architectural expressive entry canopy.

The new open space and low rise podium element provide a transition to screen the large blank parkade wall from Marine Drive. Slopes, steps and planting have been purposely integrated at the edges allowing the parkette to link seamlessly to existing sidewalk grades and to create a sense of arrival both at the new development and into the District of West Vancouver. The green space proposed reinforces the entrance into West Vancouver and provides opportunities for additional visual expression.

A 1,676 sf. (155 sm) indoor amenity space is provided in the proposal. It has an outlook to the new pedestrian mews and aperture through the building. The amenity space consists of a fitness area, a common lounge meeting room and has associated storage and washroom facilities.



Public Space, Public Life

### 09 Sustainability and Sustainable Measures

In rehabilitating the existing 660 Clyde building the proposal will endeavor to reuse as much of the existing fabric as possible. This includes the roof, main entrance glazing and some feature light fittings.

The project site benefits from strong pedestrian links to other parts of the North Shore and is well served by public transport with a range of bus services close by to downtown Vancouver and locations further West such as Horseshoe Bay. If the occupants are less reliant on cars this helps to mitigate high traffic volumes which will reduce negative impacts on air quality and the environment.

The new building will be targeting minimum compliance with Step 2 of the the BC Energy Step Code.

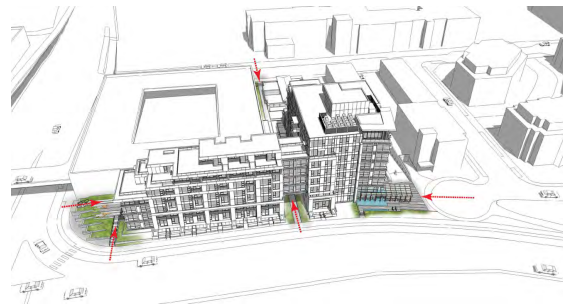
As such, the building envelope and mechanical systems of this building are designed to meet the compliance metrics of Step 2. The Thermal Energy Demand Intensity (TEDI) and Thermal Energy Use Intensity (TEUI) of this building are below 45 and 130 (kWh/sm/year), respectively. The air barrier commissioning will be conducted for this project and continuity of the air barrier will be ensured during construction. Field reviews specific to the air barrier and blower door test at mid-construction will be performed to ensure air tightness of the building. Final air tightness test will be conducted to meet requirements of the BC Energy Step Code.

In addition, the building will consider and incorporate numerous sustainable features in addition to achieving compliance with Step 2 of the BC Energy Step Code. Examples of sustainable features contemplated for the proposed development include:

- Use of local or region harvested and manufactured products
- Proximity to public transportation
- Landscape and planting on roofs to mitigate storm water run-off and reduce urban heat island effect
- Drought resistant planting to roof terraces to reduce irrigation needs
- Motion sensors and timers in public areas to reduce electrical consumption
- Efficient lights, fans, cooling/heating equipment in addition to occupant control
- Low-E glazing
- Strategies to divert construction waste from landfills
- Use of recycling material or with recycled content where possible
- Use of low emitting materials for adhesives, sealants, paints and coatings, flooring, composite wood
- Dwelling units designed to provide access to daylight and views
- Provision of operable windows in dwelling units to improve quality of indoor environment
- Use of low flow fixtures and water efficient appliances, dual flush toilets, low-flow faucets and showerheads, front load washer and water efficient dishwashers
- A high albedo roof design and finish

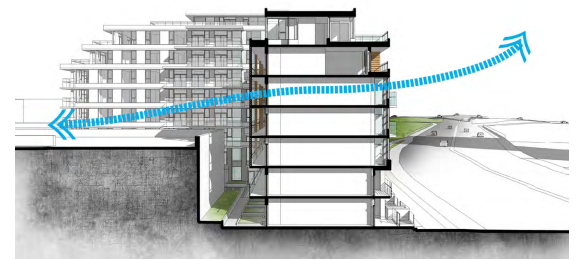
The residential mid-rise form is more sustainable approach as it creates a lower impact on the environment relative the building's footprint with sensitive increase to density. Its orientation and architectural design allows for natural light and ventilation to all habitable rooms in all suites while respecting the contextual and natural setting.

Thermally efficient window wall and curtain wall maximizes views while optimizing energy performance by allowing for cooling in the summer without negative heat gain. Single-loaded unit planning such as the podium along Marine Drive and the townhouses provide opportunities for natural and cross ventilation with circulation of fresh air through the suites. An automated VRF system is proposed for air conditioning, heating and cooling. This will improve the building's energy performance and should result in energy savings for all tenants / owners. Energy efficient elevators and energy efficient lighting with control systems will minimize electricity usage. Water use will also be reduced by installing low flow plumbing fixtures. Low VOC finishes will be specified throughout which remove any potential for off-gassing.



Connection

Various passageways through and around the building to allow residents and visitors to connect and engage with people, history and the natural setting.



Sustainable Design

Cross ventilation for units on the west podium wing with exterior walkways promoting connection with nature.

PROJECT

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DESIGN RATIONALE continued

09 Materials Board



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**A0.15**



DESIGN RATIONALE continued

10 Site Photo Montage - Potential Building Views



100 feet

50 feet



30 feet

Looking west towards Park Royal Shopping Centre and English Bay



30 feet

Looking east on Clyde Avenue towards the Capilano River

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**A0.16**

## LANDSCAPE RATIONALE

### 01 General

This is the gateway to West Vancouver and the project is respectful of the status. Landscape treatment at the base of the residential building serves as a rich setting for the modern building replete with strong urban forms at the building entry, a modest open public parkette, generous ground unit patio spaces, a courtyard with amenities for residents to utilize with garden plots, playgrounds, quiet seating areas for respite and repose. The composition is colourful, yet borrows strong natural textures and plant materials from the forested mountain backdrop to tie the project visually to the hillside beyond.

### 02 Entry Precinct

The vehicular and pedestrian entry will be an urban edge, with planter walls, striped pavers, and furnished with well-crafted amenities; bicycle storage, benches, bollards, litter containers to establish the distinctive quality of the development. The striped paving is at grade with the vehicular entry giving the impression of a grander plaza space. Water features will highlight the entry and edges of the space and create white noise to help mask the sounds of traffic along Marine Drive. Plant material has been selected to create a visual buffer from the traffic, yet provide an interesting presence to the street above the plaza. Tree placement has been carefully chosen to frame the strong geometry of the tower with tall growing Sitka Spruce.

### 03 Marine Drive

Street trees have been located along the new front boulevard that will separate pedestrians and people using the multiuse pathway from the busy street.

### 04 Executive Park

Visible from the busy intersection, the parkette showcases the development. The parkette will be elevated from the street by a series of feature stone veneer fin walls along the western perimeter, giving the space a proud sunny aspect. A sidewalk, paved with unit pavers, slopes up gently from Marine drive to a paved plaza and seating opportunities. The parkette is defined with low 600 mm retaining walls, striped paving and a foreground of a perennial garden rich with colourful seasonal plantings. The plaza will have picnic tables, benches, and litter containers, and will be lit with low-level bollard lighting. Tree planting consists of a strong evergreen grove as a backdrop to the parkette, and beech trees to buffer the wall of the adjacent parkade.

### 05 Courtyard

North of the tower we have provided amenity spaces for the residents. Wide paved pathways provide well lit pedestrian access from exits, elevators, and allows full circulation around the perimeter of the site from the entry plaza area North to Clyde Avenue and into the west parkette. Lush landscaping with small trees, a mix of broadleaf evergreen shrubs and perennials will add to the ambience of the courtyard. At the center of the courtyard will be a water feature with a slight Japanese style complementing the heritage architecture. The striped paving is also used here denoting a more park-like space. To the East is a more intimate seating area with tables and chairs for office meetings, and decorative lights overhead for the evening. To the West is another seating area with sunny aspects and an art piece providing a focal point.

### 06 Play Amenity

To the East of the site we have provided a play area which features natural play elements such as boulders and logs, and a picnic table and benches. A larger piece of play equipment is at the centre as a focal and more artistic piece. Rubber play surfacing mimics water and sand through coloured patterning to the boat themed play.

### 07 Green Roof

Extensive green roof system with a variety of drought tolerant sedums is proposed on the available roof space, providing more green for the building's users. Intensive shrub planting gives some privacy to the generous rooftop terraces.

### 08 Lighting

Bollard lights are proposed to light the key pathways across and through the site increasing safety for the evening. Street lights will light the surrounding road. Bollard lights are proposed at the Executive Park and in wall lights for the stepped entryway and stone fin walls. A cooler light is proposed at this location to accentuate the Architectural wall forms.

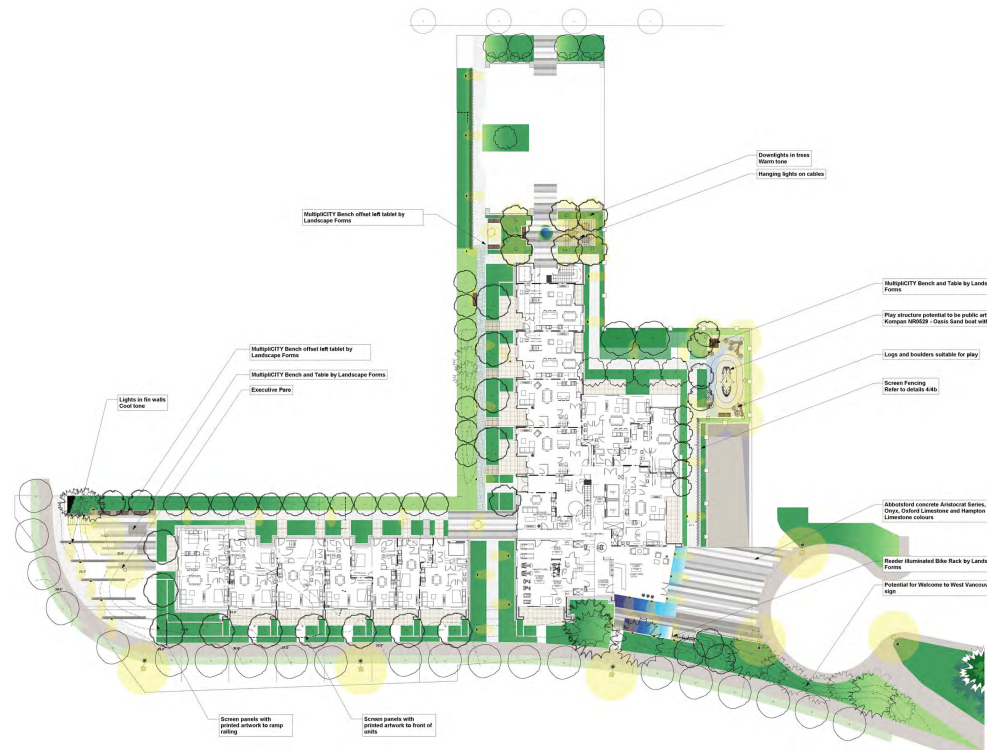
The play area is lit with bollard lighting allowing usage even in the darker evenings. Art pieces are lit with in ground lighting, the meeting area beside the courtyard has decorative warm overhead lights for a more unique and intimate atmosphere, and the water feature has space for integrated lights around the entryway area.

### 09 Public Art Opportunities

Across the site are various opportunities to incorporate art. There is opportunity to place artwork on the fin walls in the West which would be visible to the frequented road. The screen fences to the South units are proposed to be decorated with printed artwork giving them a unique character. There is space for artwork at the entry to the community facility and at the North Eastern seating area serving to provide focal points. The main play structure has opportunity to be an artistic piece, and art can also be integrated into the water features.

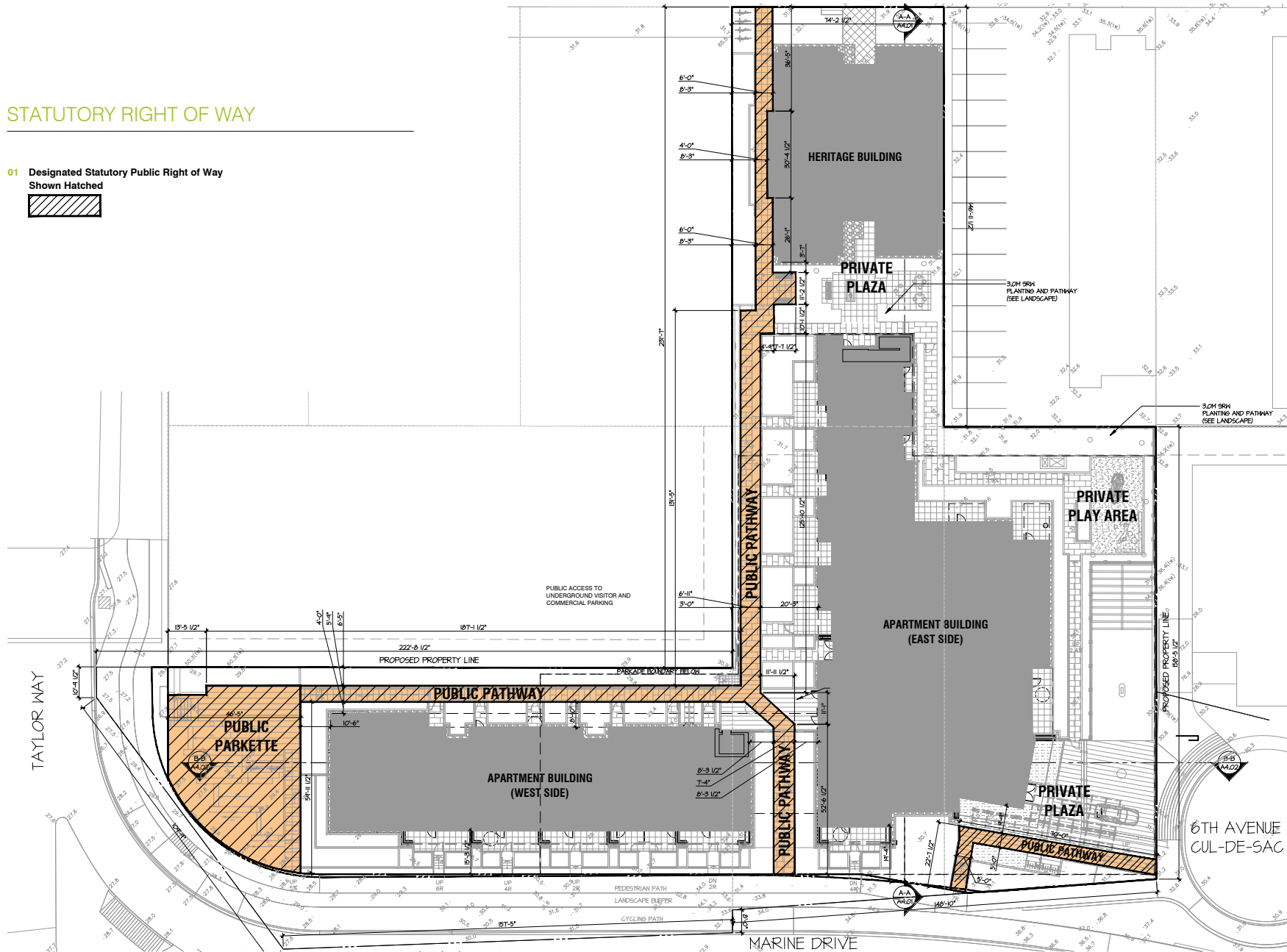
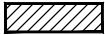
### 10 Planting

Overall the planting of the site moves from perennials offering a showcase effect at the Executive park, to more formal planting outside the units; planting respectful of the old heritage building and style and more naturalised planting to the East to marry into the site's eastern context.



# STATUTORY RIGHT OF WAY

01 Designated Statutory Public Right of Way  
Shown Hatched



PLOT STAMP: 2021-Jul-28 @9:12am - P:\A215294 - Executive Group - Park Royal Site\CAD\SD\A215294\_Rezoning.dwg - A0.18

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| 04  | 2020.02.25 | REISSUED FOR CITY REVIEW    |
| 05  | 2020.05.22 | REISSUED FOR CITY REVIEW    |
| 06  | 2020.06.16 | REISSUED TO CITY - COVENANT |

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PROJECT

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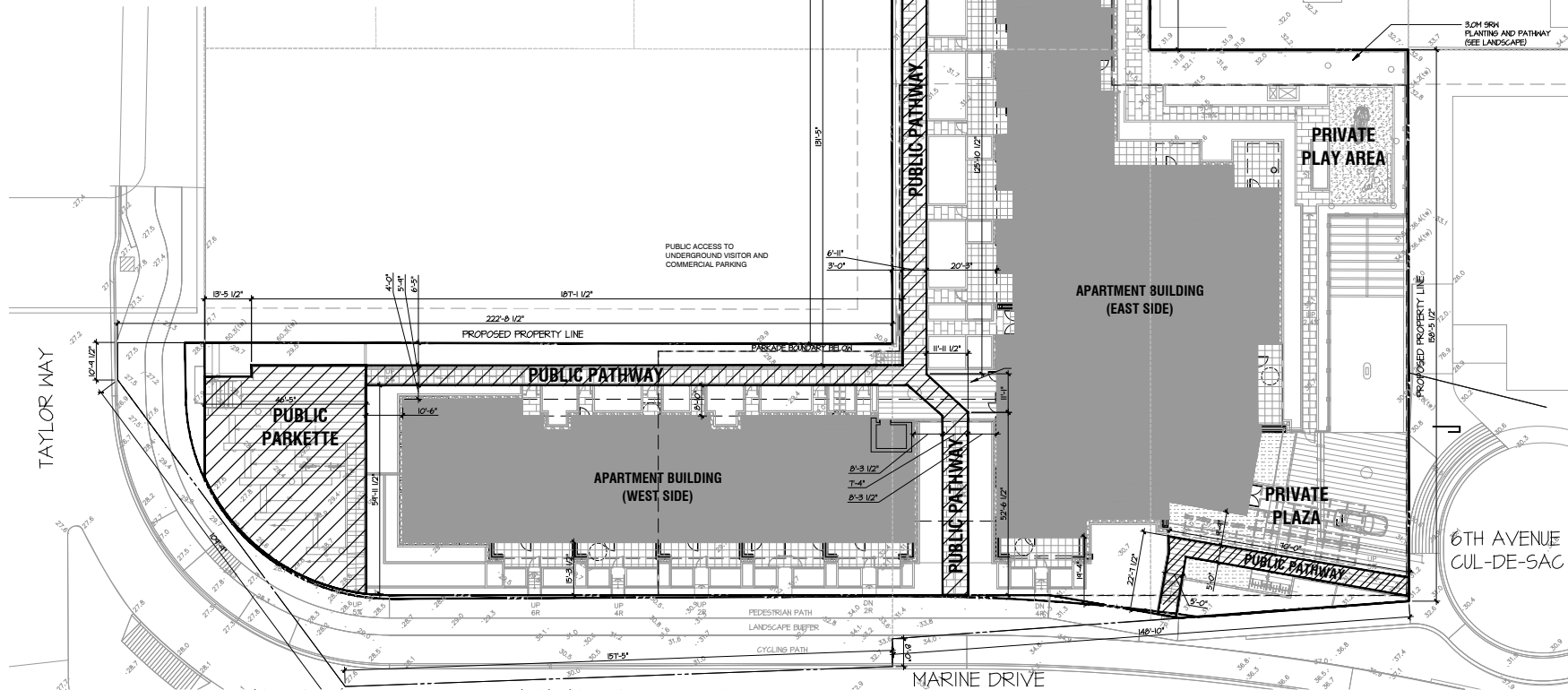
SCALE 1/16" = 1'-0"  
DATE FEB. 25, 2020

**A0.18**



# STATUTORY RIGHT OF WAY

01 Location of Parks and Pathways



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**LOCATION OF  
PARKS + PATHWAYS**

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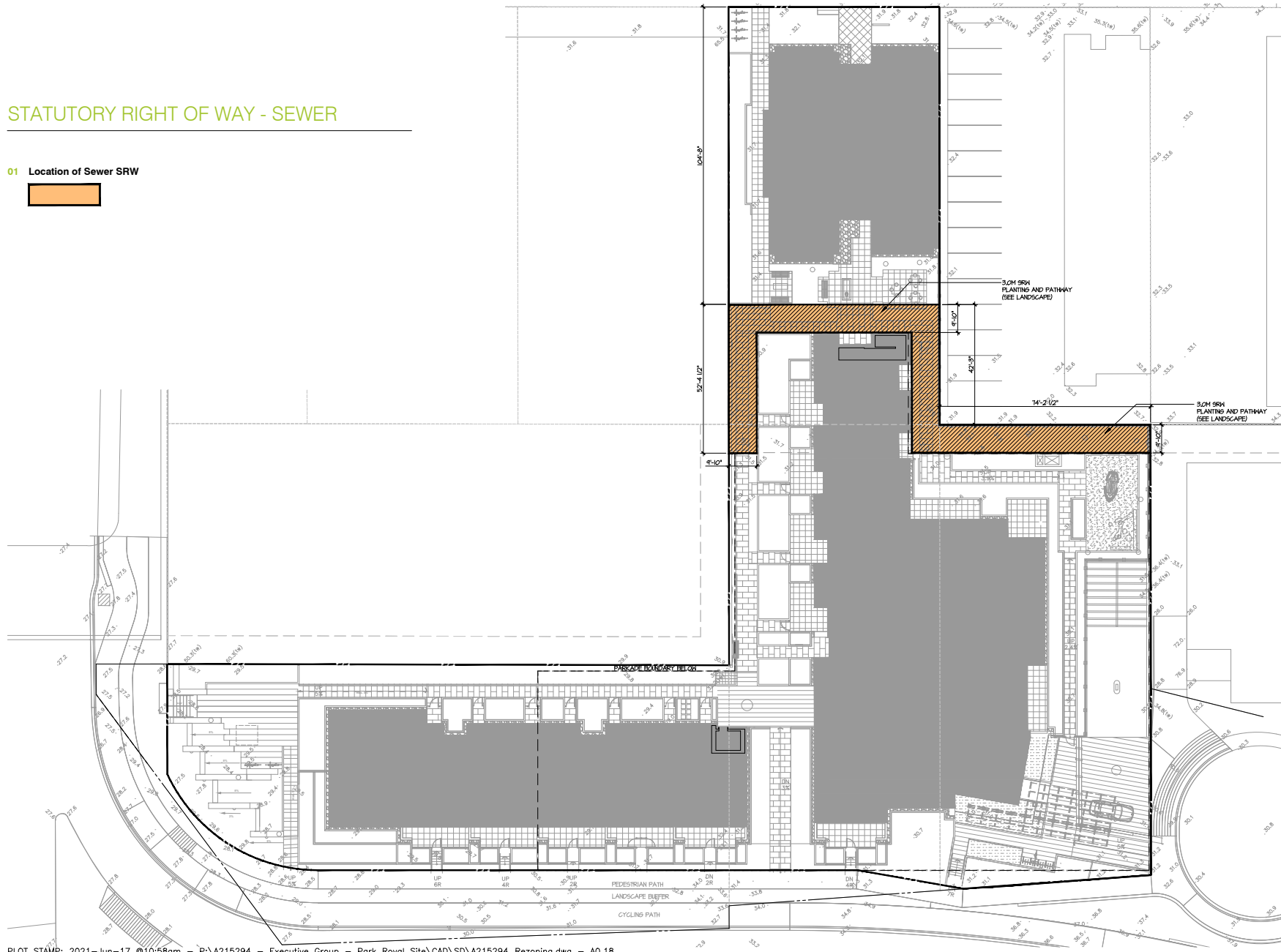
PROJECT A215294  
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SCALE 1/16" = 1'-0"  
DATE AUG. 25, 2020

**A0.18C**

# STATUTORY RIGHT OF WAY - SEWER

01 Location of Sewer SRW



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**LOCATION OF  
 SEWER SRW**

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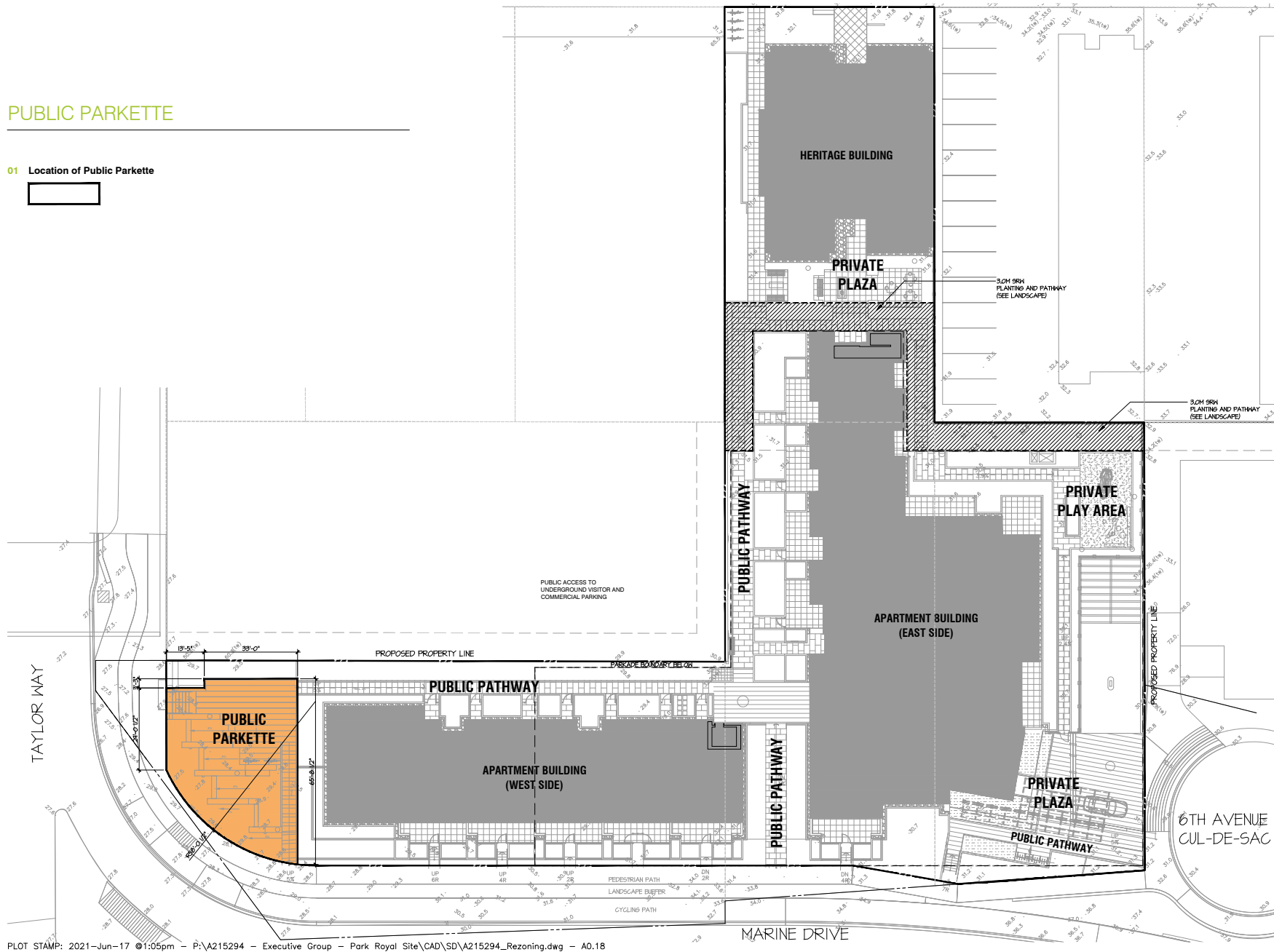
SCALE 1/16" = 1'-0"  
 DATE AUG. 25, 2020

**A0.18d**



# PUBLIC PARKETTE

01 Location of Public Parkette



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**LOCATION OF  
 PUBLIC PARKETTE**

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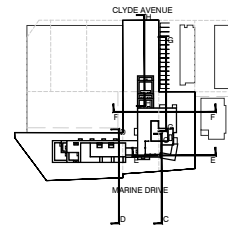
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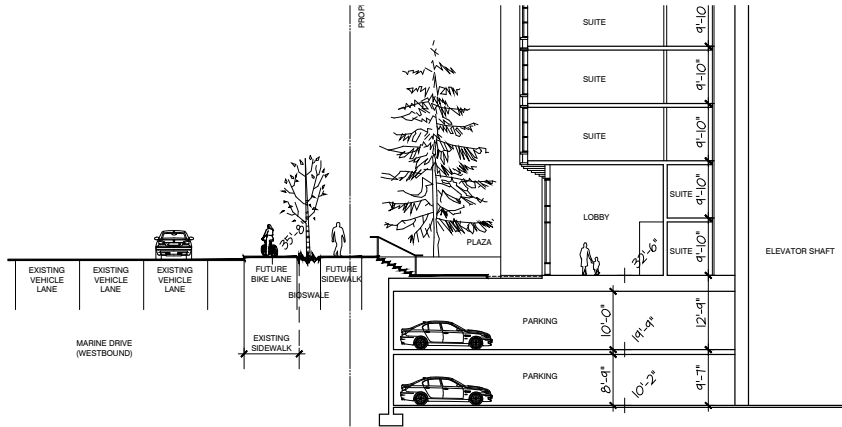
**A0.18e**

# PUBLIC REALM

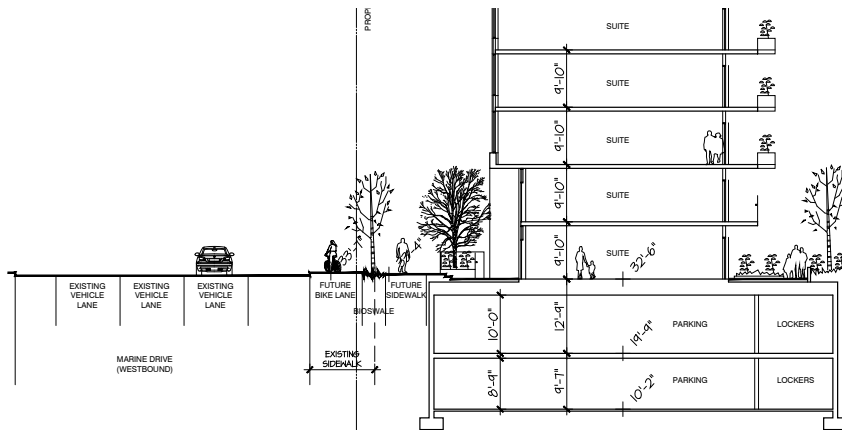
## 02 Partial Sections and Related Perspectives



Key Plan



Section C-C Through Public Boulevard and Entrance Lobby



Section D-D Through Public Boulevard and Residential Podium



Entry Plaza View Looking South West at Entrance Lobby



Pedestrian Boulevard at Podium View Looking West down Marine Drive

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**PUBLIC REALM**

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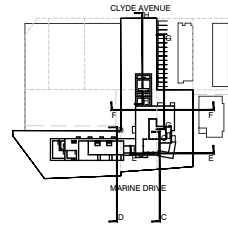
SCALE N.T.S.  
DATE NOV. 19, 2018



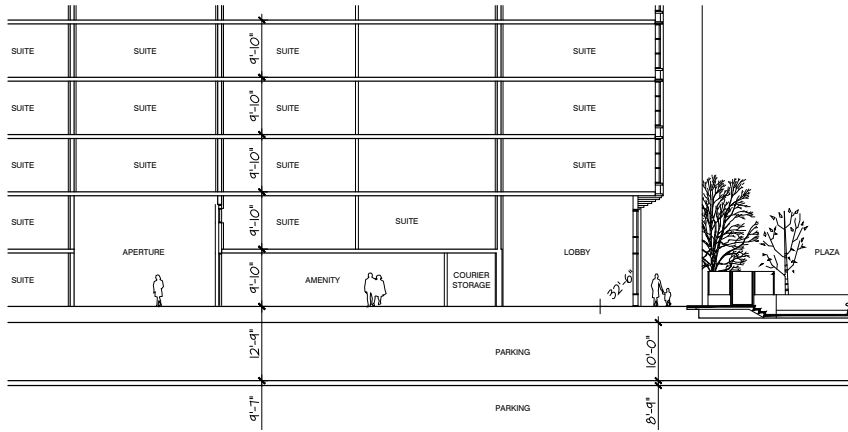
**A0.19**

PUBLIC REALM continued

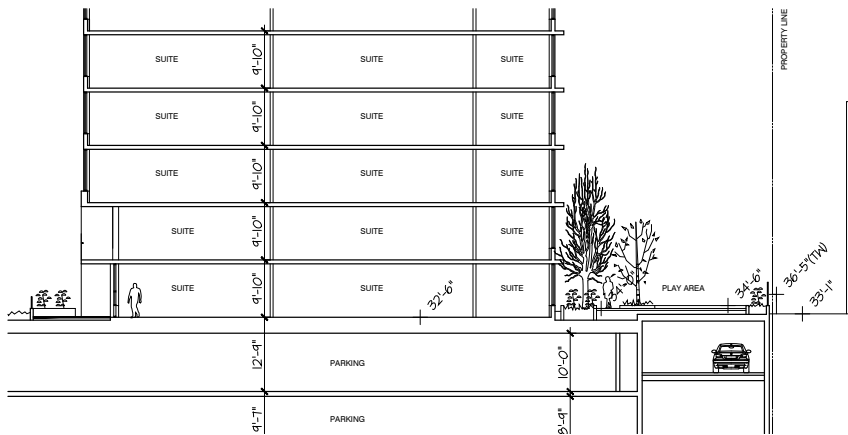
03 Partial Sections and Related Perspectives continued



Key Plan



Section E-E Through Lobby and Entry Plaza



Section F-F Through Pedestrian Throughway and Residential Tower



Entry Plaza Aerial View



Pedestrian Throughway View Looking North towards Heritage Building

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**EXECUTIVE**  
GROUP DEVELOPMENT

NO.	DATE	ISSUE
01	2018.05.17	ISSUED FOR DRC
02	2018.11.19	ISSUED FOR PCM
03	2019.03.28	RESPONSE TO MEMORANDUM

NO. | DATE | REVISION

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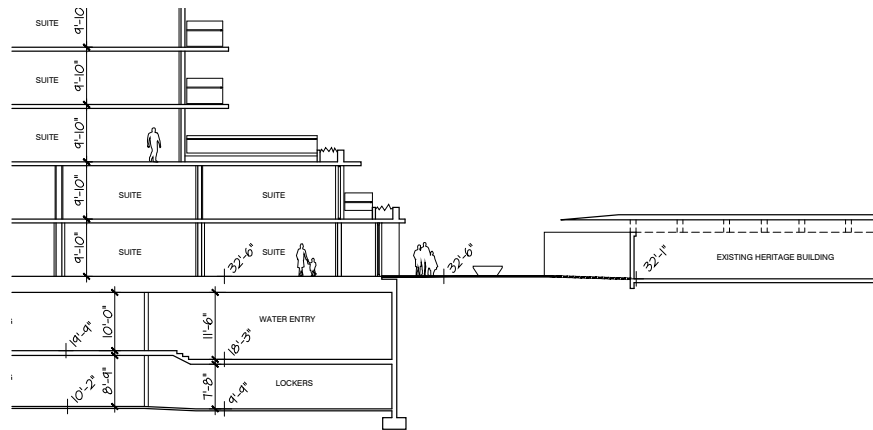
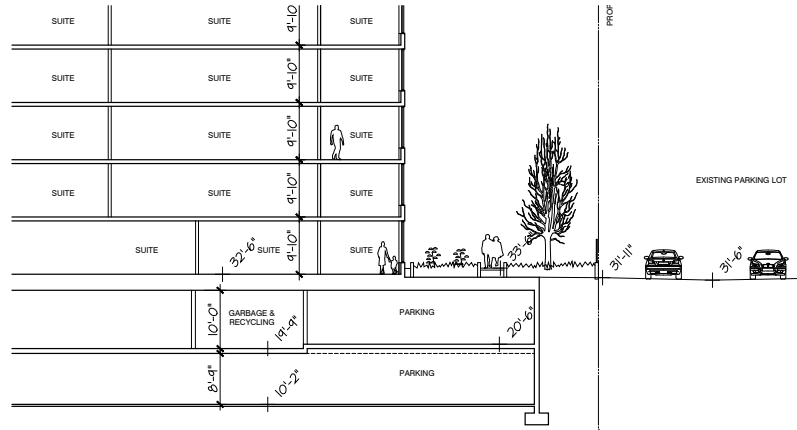
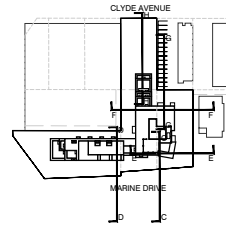


**A0.20**



PUBLIC REALM continued

04 Partial Sections and Related Perspectives continued



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**A0.21**

PUBLIC REALM continued

05 Public Parkette and Urban Edge Perspectives



Entering West Vancouver View Looking North-West



Urban Edge Street View Looking North into Pedestrian Mews Connection to Heritage Building



Public Parkette View Looking East



Leaving West Vancouver View Looking North-East

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**A0.22**



# PRECEDENTS

## 01 Precedents



Ohara Residence Los Angeles



Urban Mews London, England



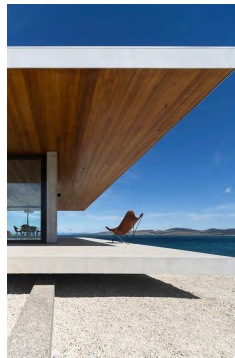
Evelyn West Vancouver, BC



Tapestry Wesbrook Village, UBC



Georgian Rowhouse London, England



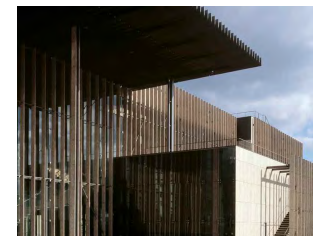
Dunalley House Tasmania, Australia



MOMA - Museum of Modern Art New York City, New York



West Coast Modern Various Locations



Nagasaki Art Museum Japan

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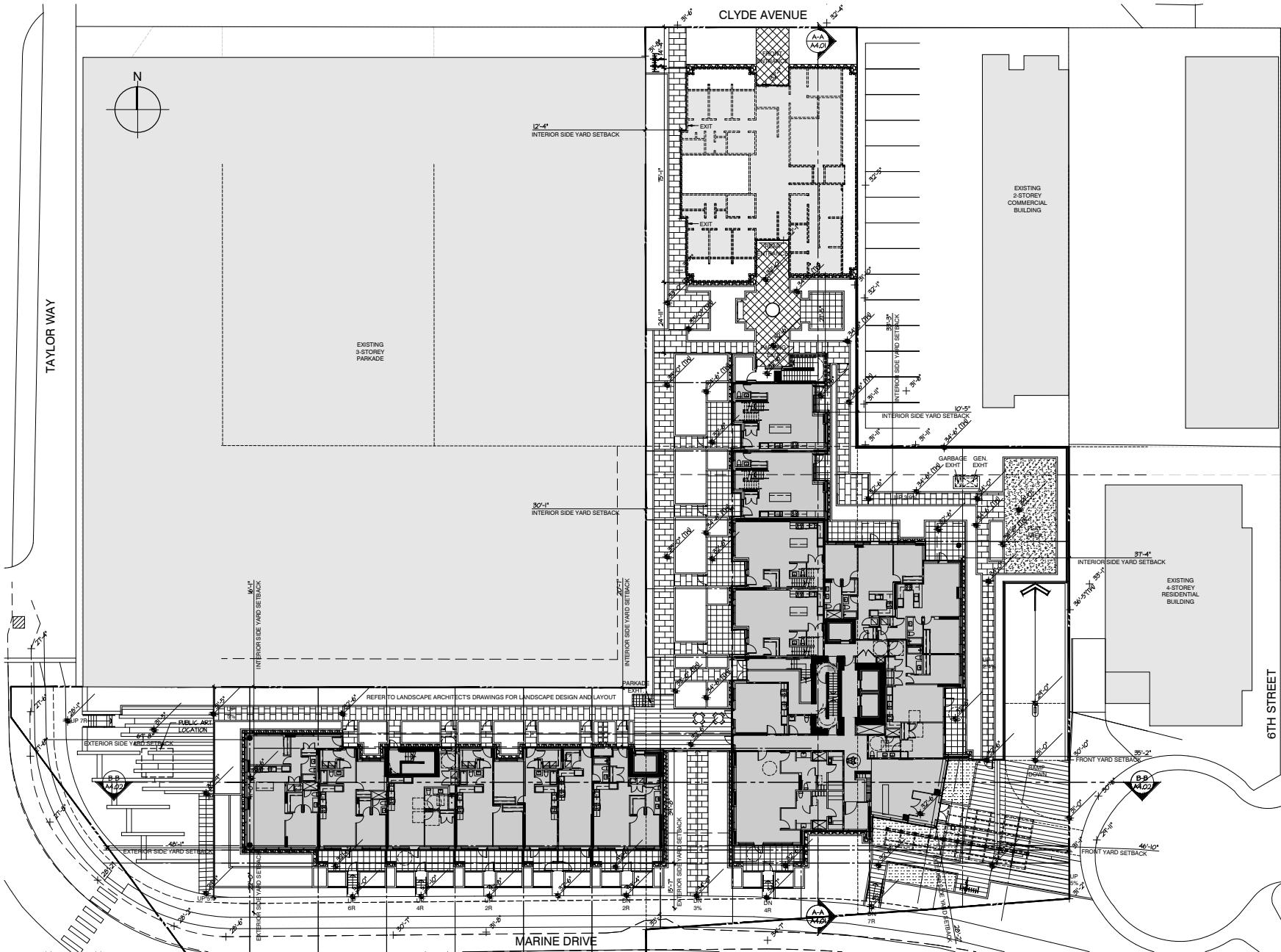
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**A0.23**



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**SITE PLAN**

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SCALE 1/16"=1'-0"  
 DATE NOV. 19, 2018

**A1.01**

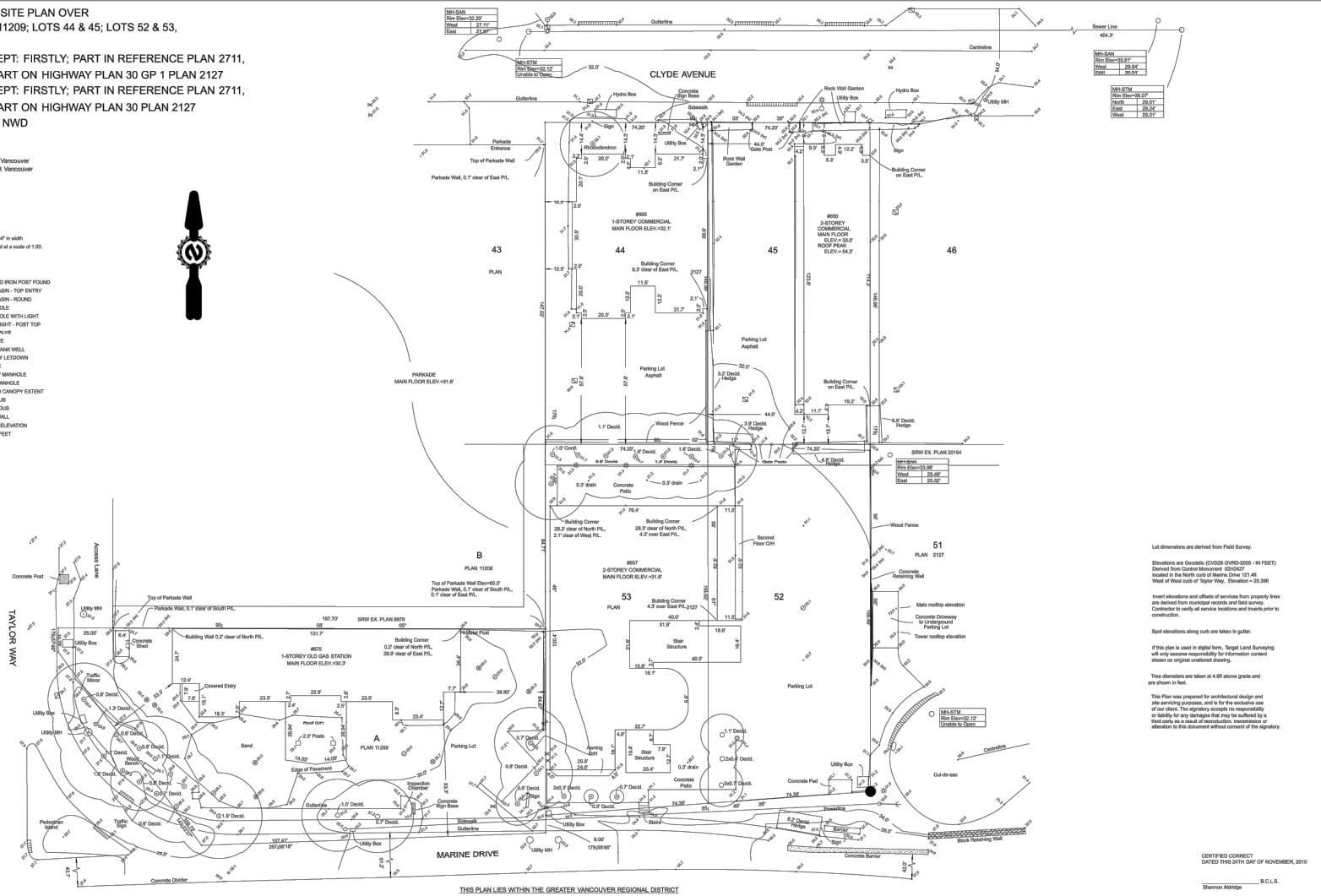
**TOPOGRAPHIC SITE PLAN OVER**  
**1) LOT A PLAN 11209; LOTS 44 & 45; LOTS 52 & 53,**  
**2) LOTS 44 & 45**  
**3) LOT 53, EXCEPT: FIRSTLY; PART IN REFERENCE PLAN 2711,**  
**SECONDLY; PART ON HIGHWAY PLAN 30 GP 1 PLAN 2127**  
**3) LOT 52, EXCEPT: FIRSTLY; PART IN REFERENCE PLAN 2711,**  
**SECONDLY; PART ON HIGHWAY PLAN 30 PLAN 2127**  
**ALL OF DL 1039 NWD**

**CIVIC ADDRESS:**  
 857, 875 Marine Drive, West Vancouver  
 855, 856 Clyde Avenue, West Vancouver  
 PID: 009-125-680  
 PID: 014-020-786  
 PID: 008-174-849  
 PID: 014-020-848  
 PID: 014-020-958

**SCALE: 1" = 20'**  
 ALL DIMENSIONS ARE IN FEET  
 The intended plot size of this plan is 30" in width  
 and 22" in height (B) when plotted at a scale of 1:20.

- LEGEND**
- DENOTES STANDARD IRON POST FOUND
  - DENOTES CATCH BASIN - TOP ENTRY
  - DENOTES CATCH BASIN - ROUND
  - DENOTES POWER POLE
  - DENOTES POWER POLE WITH LIGHT
  - DENOTES STREET LIGHT - POST TOP
  - DENOTES WATER VALVE
  - DENOTES GAS VALVE
  - DENOTES PETROL TANK WELL
  - DENOTES DRAINAGE LETDOWN
  - DENOTES GUY WIRE
  - DENOTES SANITARY MANHOLE
  - DENOTES STORM MANHOLE
  - DENOTES TREE AND CANOPY EXTENT
  - DENOTES DECIDUOUS
  - DENOTES CONIFEROUS
  - DENOTES TOP OF WALL
  - DENOTES GROUND ELEVATION
  - DENOTES SQUARE FEET

Lot Area	Sub A	Sub B	Sub C
Lot 44	10980	8117	10980
Lot 45	10980	8117	10980
Lot 52	10980	8117	10980
Lot 53	10980	8117	10980
Total	84770	8117	



Lot dimensions are derived from Field Survey.

Elevations are Contours (CN209 OVER3005 - IN FEET)  
 Derived from Control Measurement 02-0427  
 located in the North end of Marine Drive 121-48  
 West of West end of Taylor Way. Elevation = 25.396

Invert elevations and offsets of services from property lines  
 are derived from municipal records and field survey.  
 Contractor to verify all service locations and inverts prior to  
 construction.

Spot elevations along curb are taken in gutter.

If this plan is used in digital form, Target Land Surveying  
 will only ensure responsibility for information content  
 shown on original unaltered drawing.

This drawing is taken at 4.88 above grade and  
 are shown in feet.

This Plan was prepared for architectural design and  
 site servicing purposes, and is for the exclusive use  
 of our client. The signatory accepts no responsibility  
 or liability for any damages that may be suffered by a  
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FILE: N1401-TOPO.PLT  
 © TARGET LAND SURVEYING 2010

CERTIFIED CORRECT  
 DATED THIS 24TH DAY OF NOVEMBER, 2010  
 B.C.L.S.  
 Sherman Ashbridge

THIS PLAN LIES WITHIN THE GREATER VANCOUVER REGIONAL DISTRICT

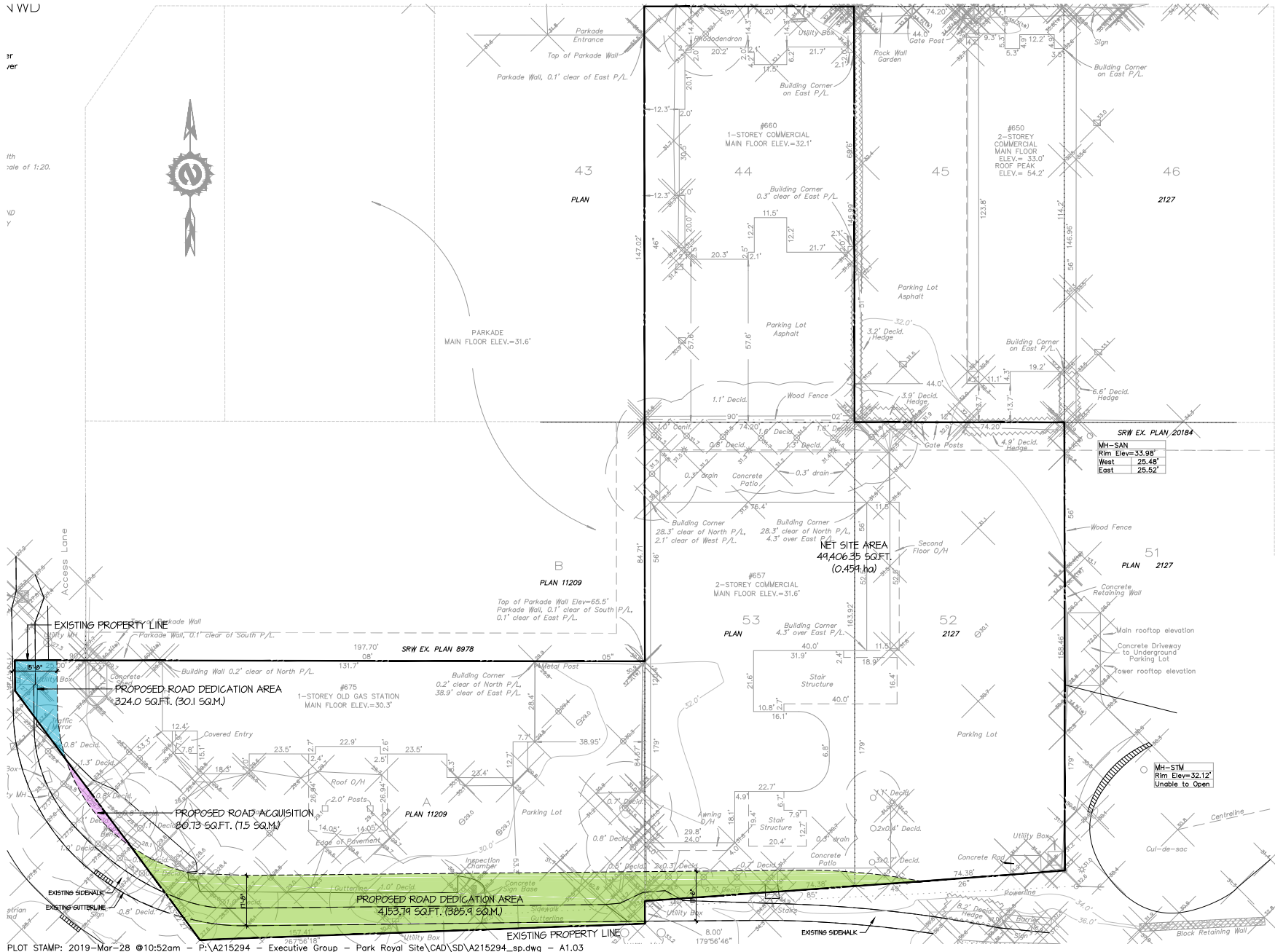


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**dys** architecture

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**PROJECT**  
**EXECUTIVE PARC**  
**MARINE+TAYLOR RESIDENTIAL**

657 Marine Drive  
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**SITE PLAN**  
**AREA & DEDICATIONS**

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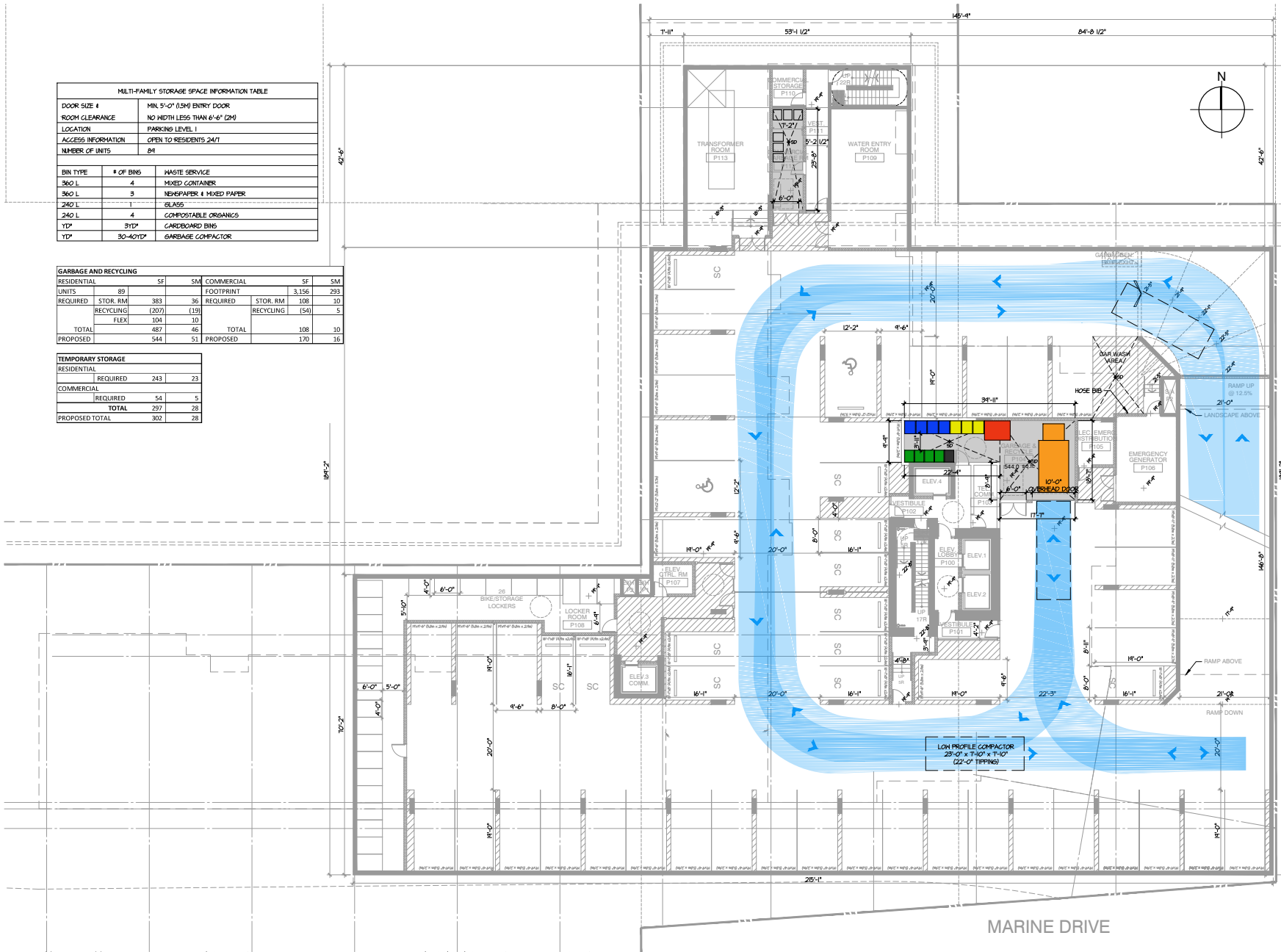
SCALE 1/16"=1'-0"  
DATE MARCH 27, 2019

**A1.03**

MULTI-FAMILY STORAGE SPACE INFORMATION TABLE		
DOOR SIZE & ROOM CLEARANCE	MIN. 5'-0" (1.5M) ENTRY DOOR. NO WIDTH LESS THAN 6'-6" (2M)	
LOCATION	PARKING LEVEL 1	
ACCESS INFORMATION	OPEN TO RESIDENTS 24/7	
NUMBER OF UNITS	84	
BIN TYPE	# OF BINS	WASTE SERVICE
360 L	4	MIXED CONTAINER
360 L	3	NEWSPAPER & MIXED PAPER
240 L	1	GLASS
240 L	4	COMPOSTABLE ORGANICS
YD*	3YD*	CARDBOARD BINS
YD*	30-40YD*	GARBAGE COMPACTOR

GARBAGE AND RECYCLING					
RESIDENTIAL	SF	SM	COMMERCIAL	SF	SM
UNITS	89		FOOTPRINT	3,156	293
REQUIRED	STOR. RM. 383	36	REQUIRED	STOR. RM. 108	10
	RECYCLING (207)	(19)		RECYCLING (54)	5
	FLEX 104	10			
TOTAL	487	46	TOTAL	108	10
PROPOSED	544	51	PROPOSED	170	16

TEMPORARY STORAGE		
RESIDENTIAL	REQUIRED	243
		23
COMMERCIAL	REQUIRED	54
		5
TOTAL	REQUIRED	297
		28
PROPOSED TOTAL		302
		28



**BIN TYPES**

- MIXED CONTAINER
- NEWSPAPER / MIXED PAPER
- GLASS
- COMPOSTABLE ORGANICS
- CARDBOARD BIN
- COMPACTOR
- TEMP. STORAGE AREA

**PROJECT**  
**EXECUTIVE PARC MARINE+TAYLOR RESIDENTIAL**  
 657 Marine Drive West Vancouver BC  
**SITE PLAN GARBAGE & RECYCLING**

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**BIN TYPES**

- MIXED CONTAINER
- NEWSPAPER/ MIXED PAPER
- GLASS
- COMPOSTABLE ORGANICS
- CARDBOARD BIN
- COMPACTOR
- TEMP. STORAGE AREA

**PROJECT**  
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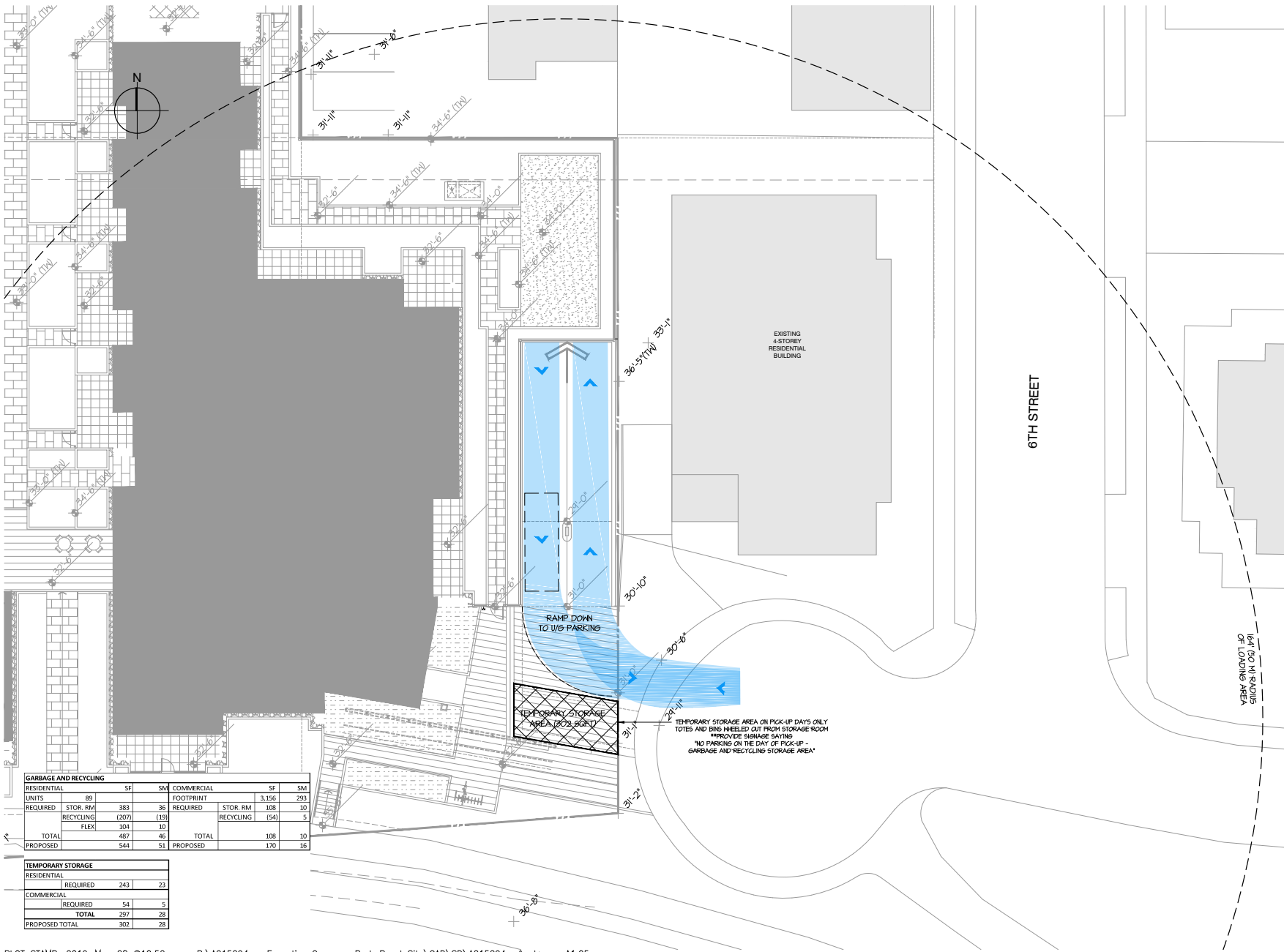
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 DATE NOV. 19, 2018

**A1.05**



**GARBAGE AND RECYCLING**

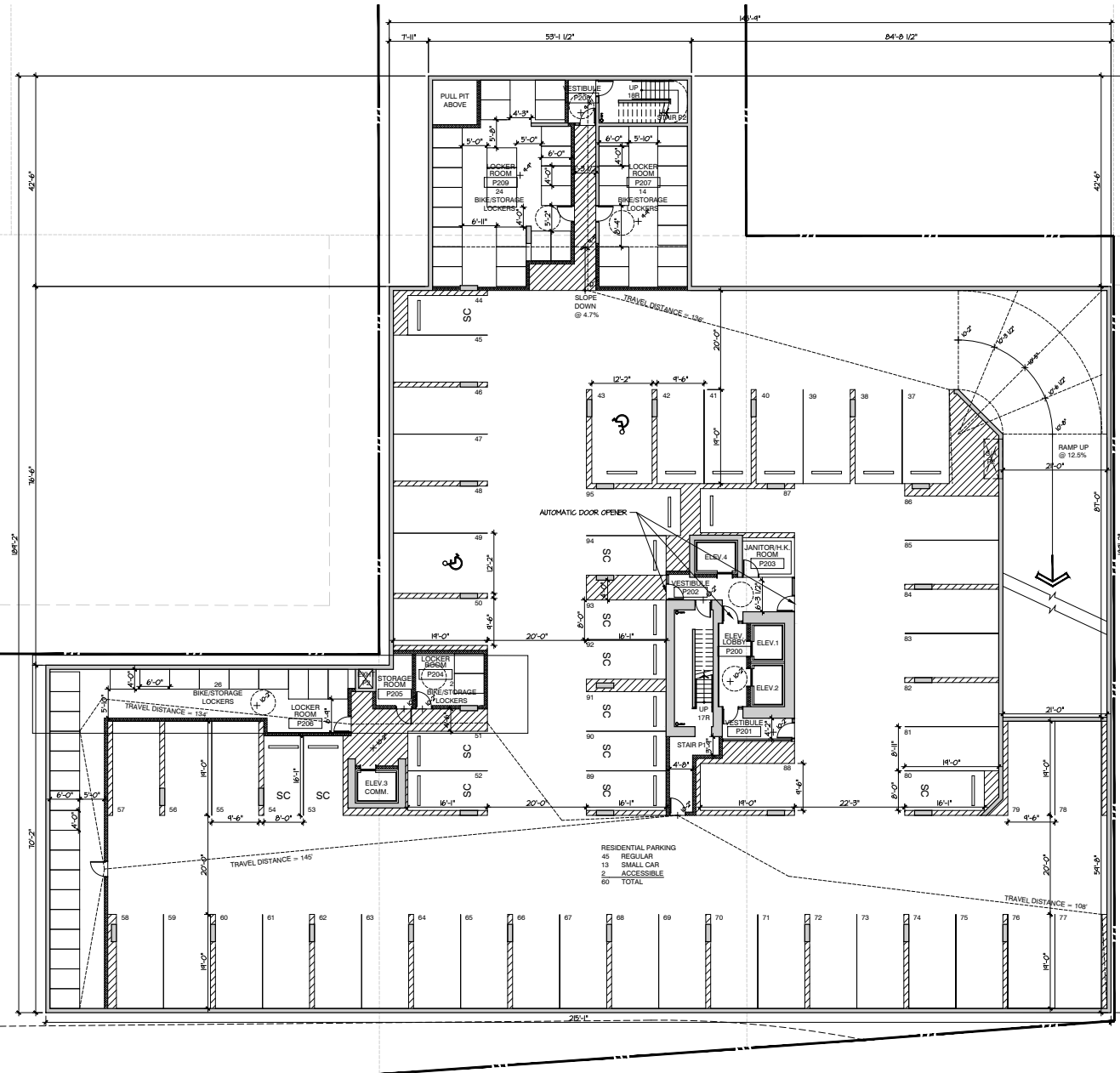
RESIDENTIAL	SF	SM	COMMERCIAL	SF	SM
UNITS	89		FOOTPRINT	3,156	293
REQUIRED STOR. RM	383	36	REQUIRED STOR. RM	108	10
RECYCLING	(207)	(19)	RECYCLING	(54)	5
FLEXI	104	10			
TOTAL	487	46	TOTAL	108	10
PROPOSED	544	51	PROPOSED	170	16

**TEMPORARY STORAGE**

RESIDENTIAL	REQUIRED	243	23
COMMERCIAL	REQUIRED	54	5
TOTAL	297	28	
PROPOSED TOTAL	302	28	

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RESIDENTIAL PARKING

45	REGULAR
13	SMALL CAR
2	ACCESSIBLE
60	TOTAL

**PROJECT**  
**EXECUTIVE PARC**  
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**PARKING PLAN**  
**LEVEL P2**

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**A2.01**

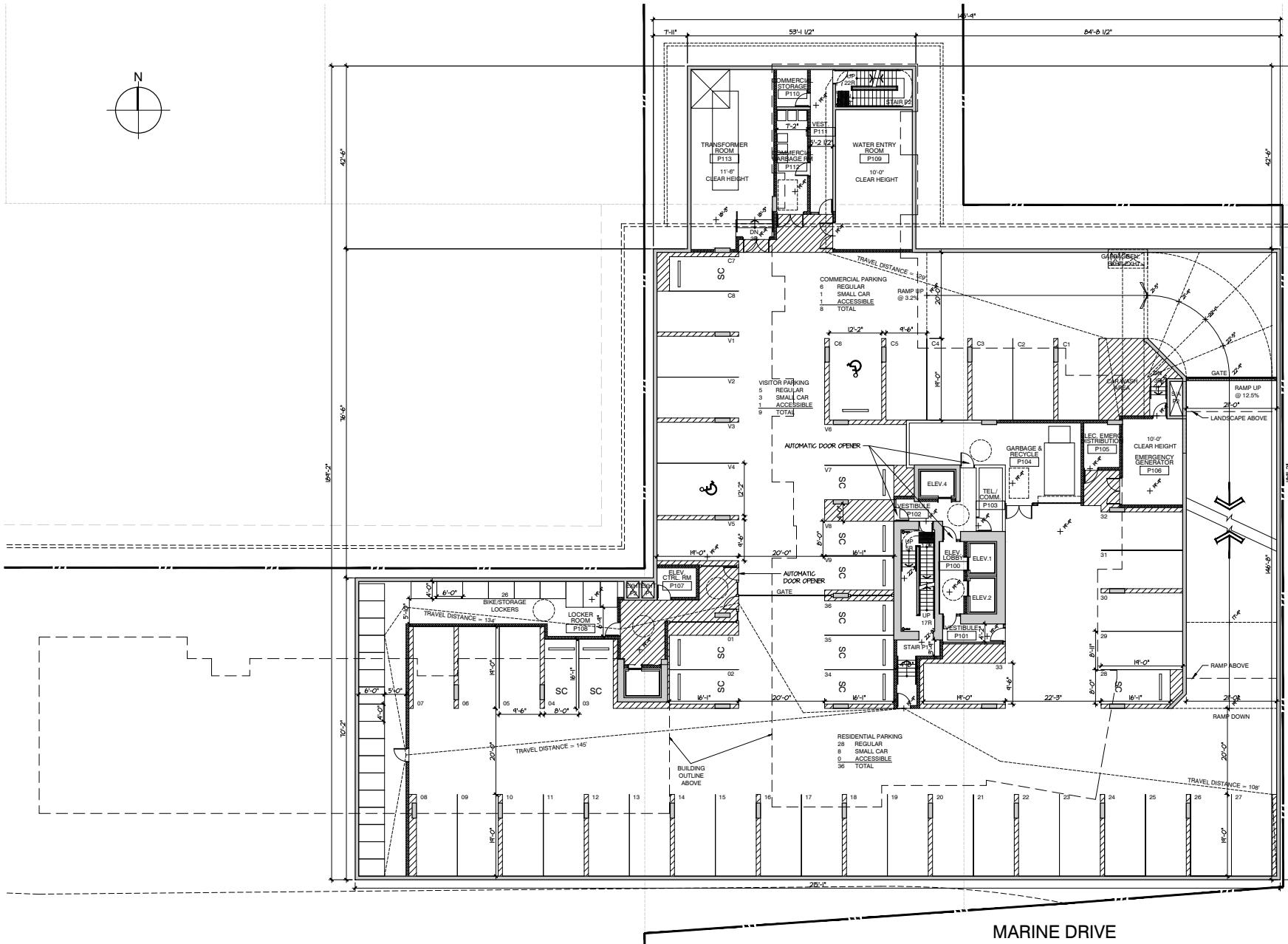
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**PARKING PLAN**  
**LEVEL P1**

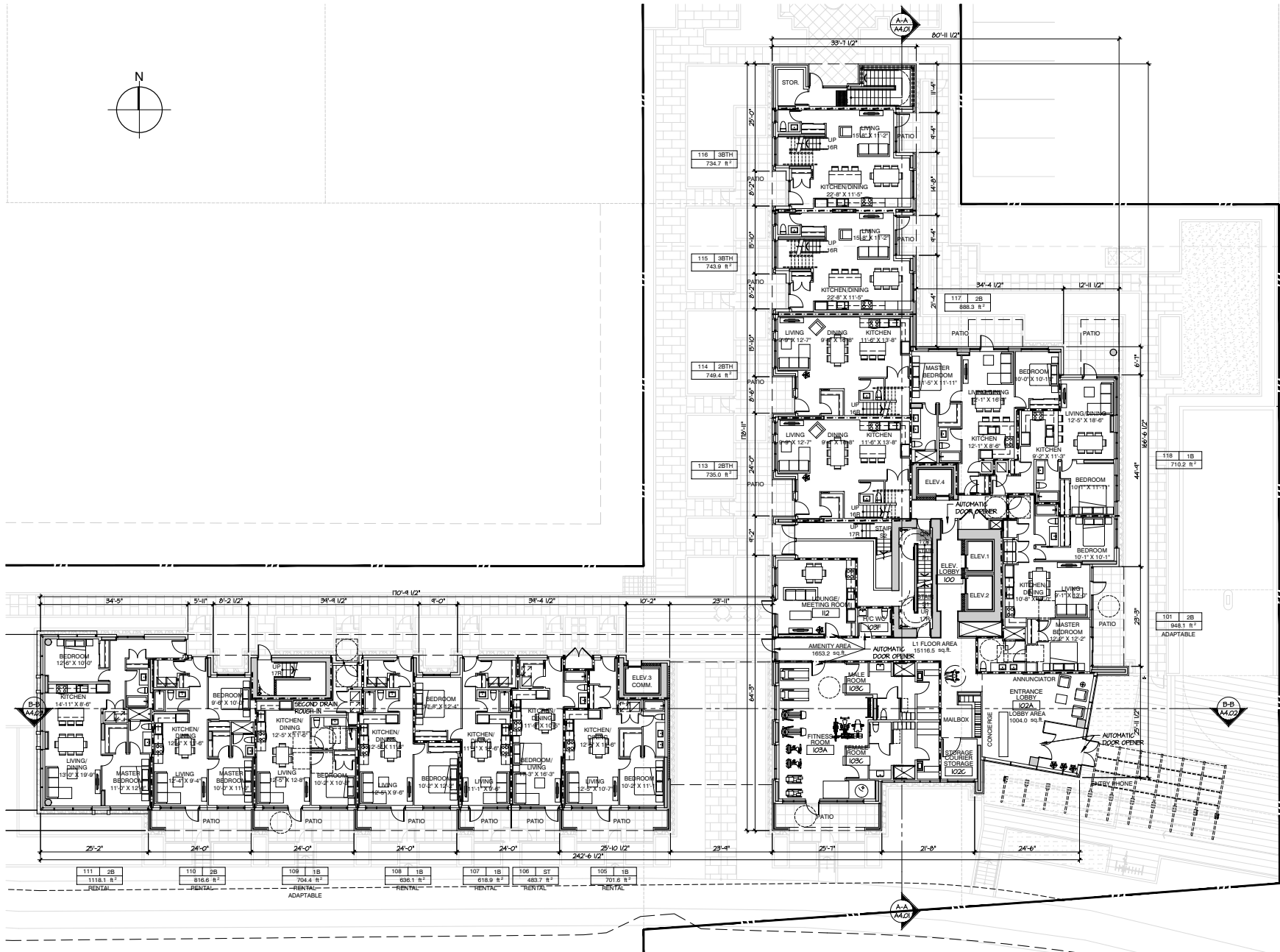
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**A2.02**



**PROJECT**  
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**FLOOR PLAN LEVEL 1**

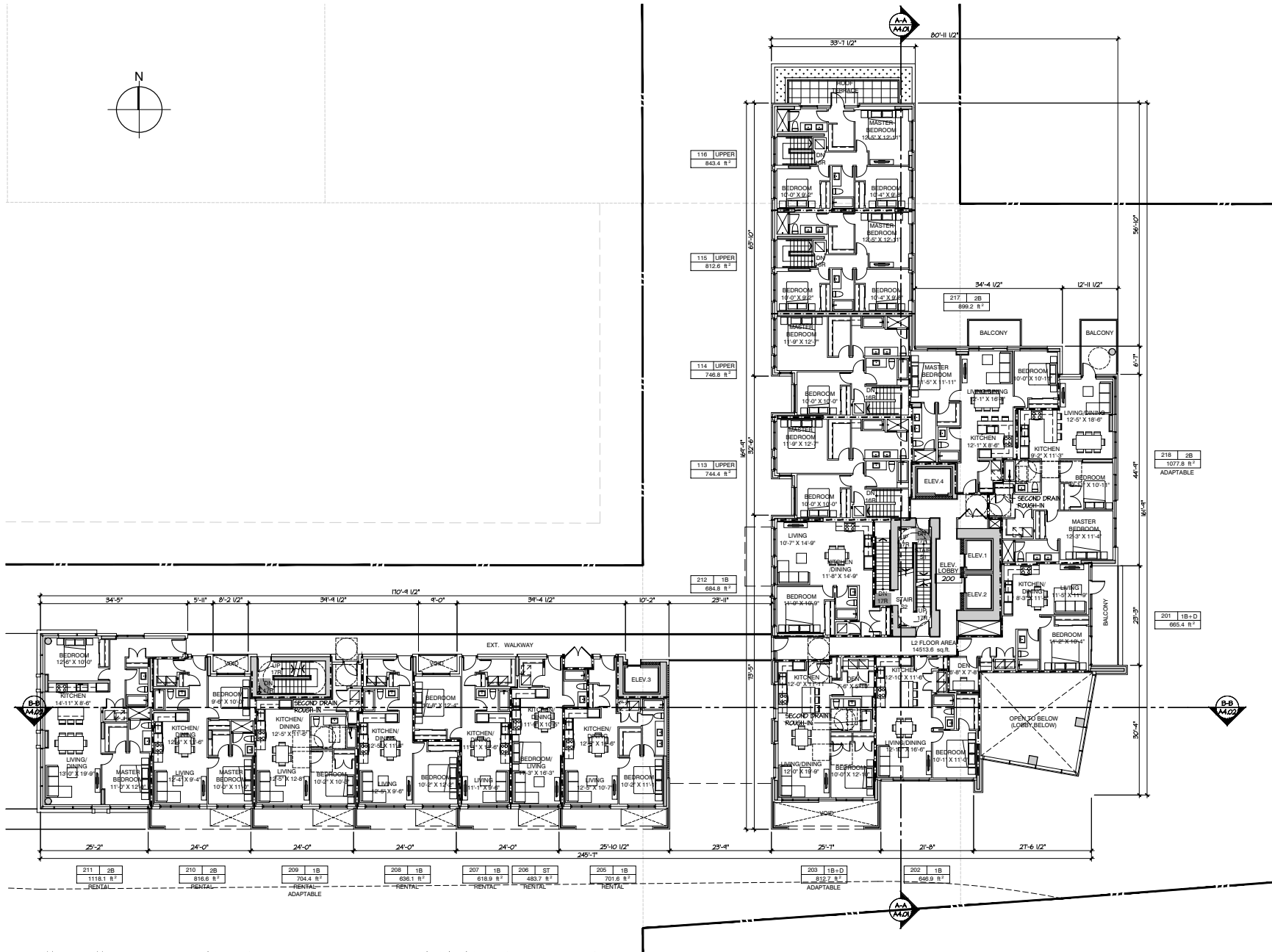
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**A2.03**





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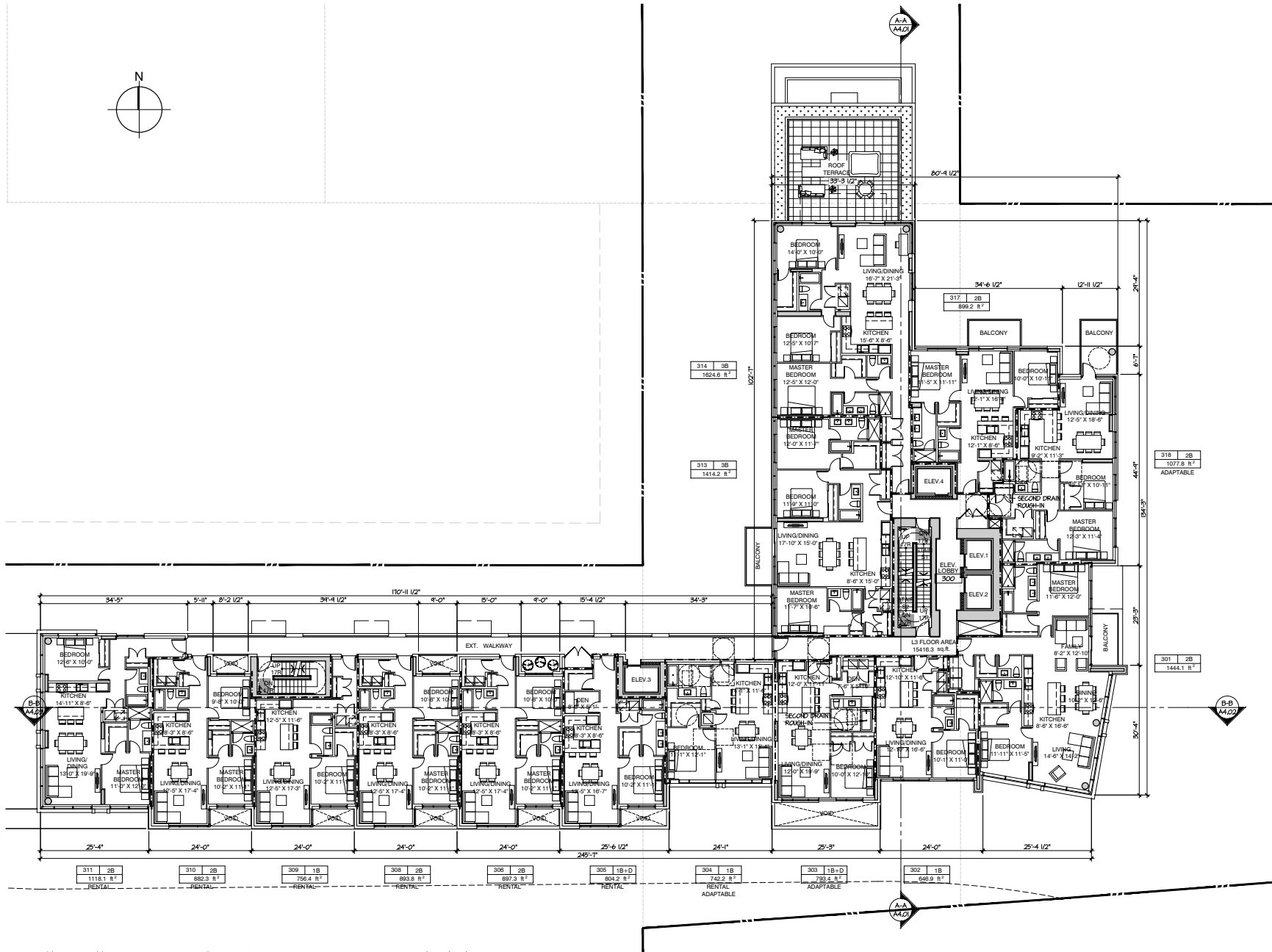
**FLOOR PLAN LEVEL 2**

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**A2.04**



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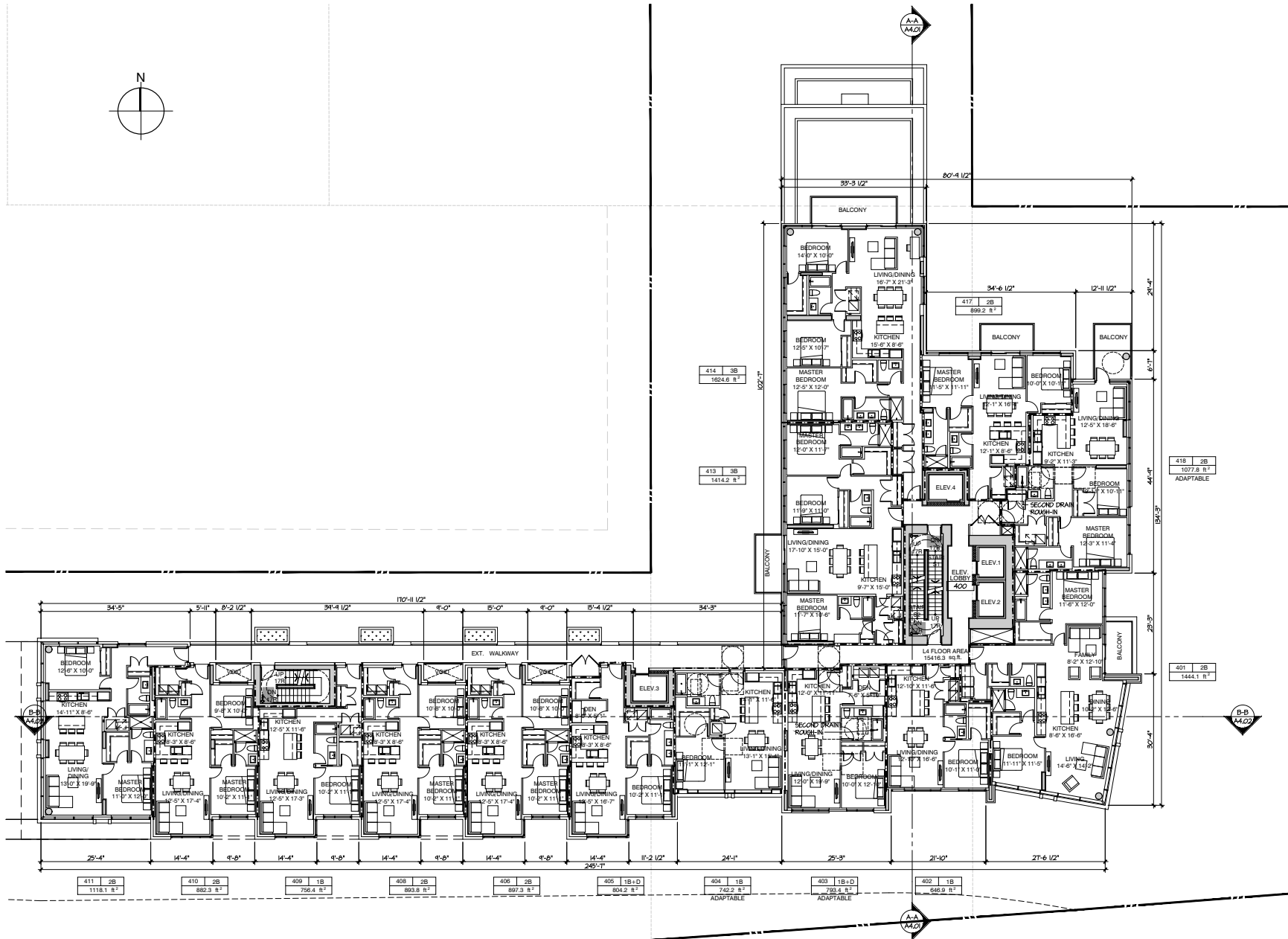
657 Marine Drive  
 West Vancouver BC

**FLOOR PLAN LEVEL 3**

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PROJECT A215294  
 DRAWN JL CHECKED NC  
 SCALE 3/32"=1'-0"  
 DATE NOV. 19, 2018



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**EXECUTIVE**  
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 03 | 2019.03.28 | RESPONSE TO MEMORANDUM

NO. | DATE | REVISION

**PROJECT**  
 EXECUTIVE PARC  
 MARINE+TAYLOR RESIDENTIAL

657 Marine Drive  
 West Vancouver BC

**FLOOR PLAN**  
 LEVEL 4

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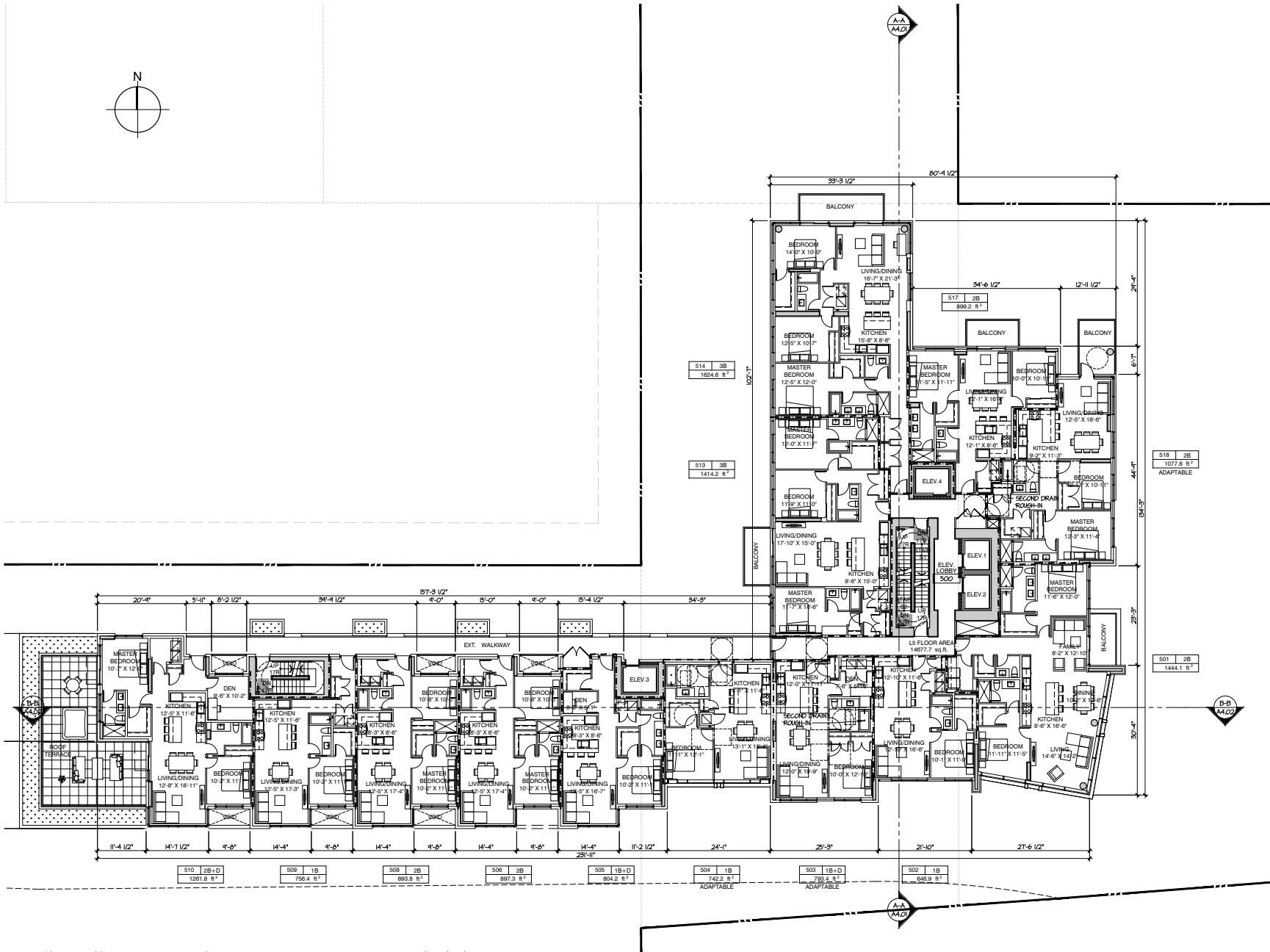
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PROJECT A215294  
 DRAWN JL CHECKED NC

SCALE 3/32"=1'-0"  
 DATE NOV. 19, 2018

**A2.06**





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PROJECT  
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**MARINE+TAYLOR RESIDENTIAL**

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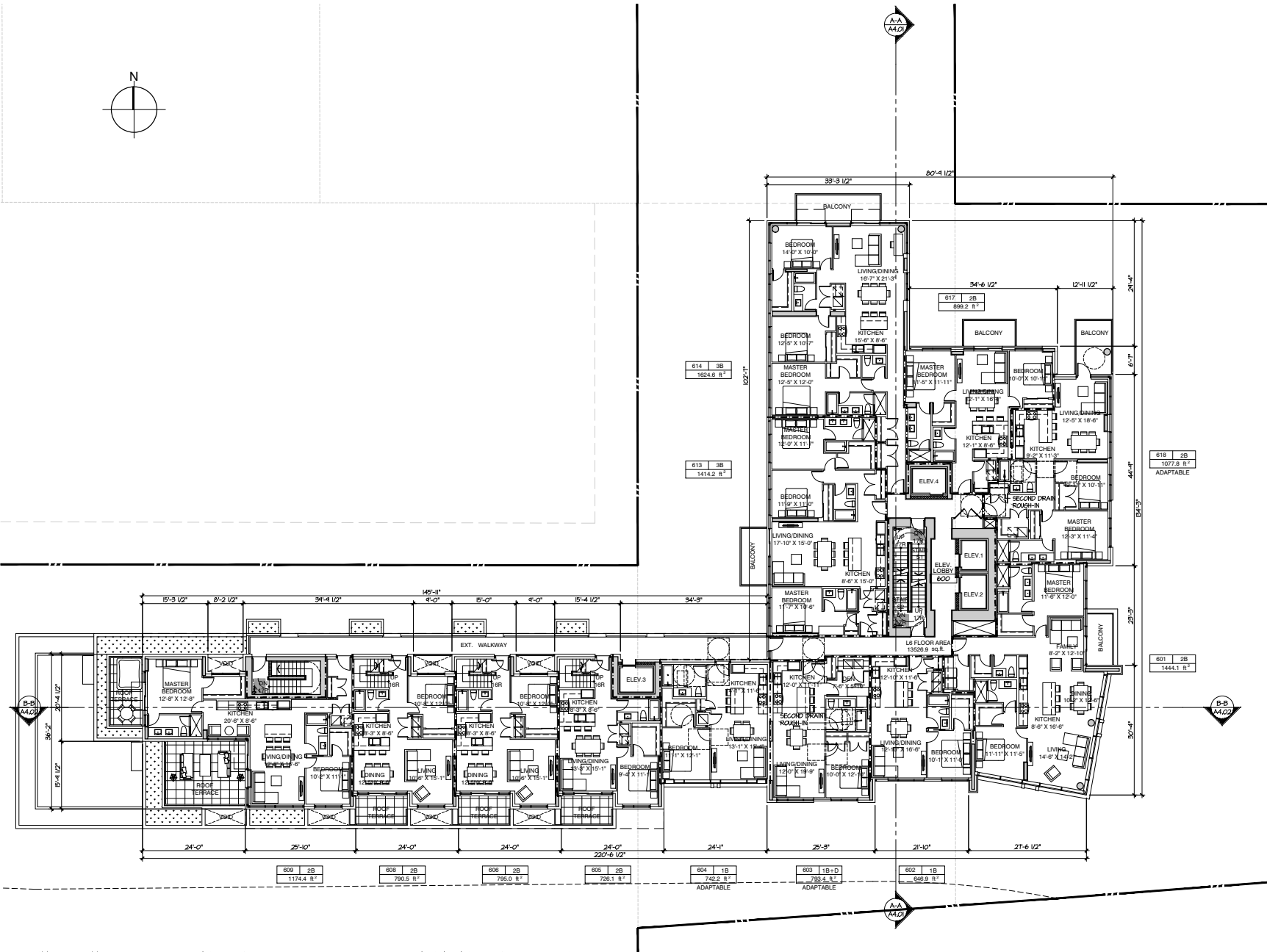
**FLOOR PLAN**  
**LEVEL 5**

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 DATE NOV. 19, 2018

**A2.07**



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**PROJECT**  
**EXECUTIVE TAYLOR MARINE+TAYLOR RESIDENTIAL**

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 West Vancouver BC

**FLOOR PLAN LEVEL 6**

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 DRAWN JL CHECKED NG  
 SCALE 3/32"=1'-0"  
 DATE NOV. 19, 2018

**A2.08**



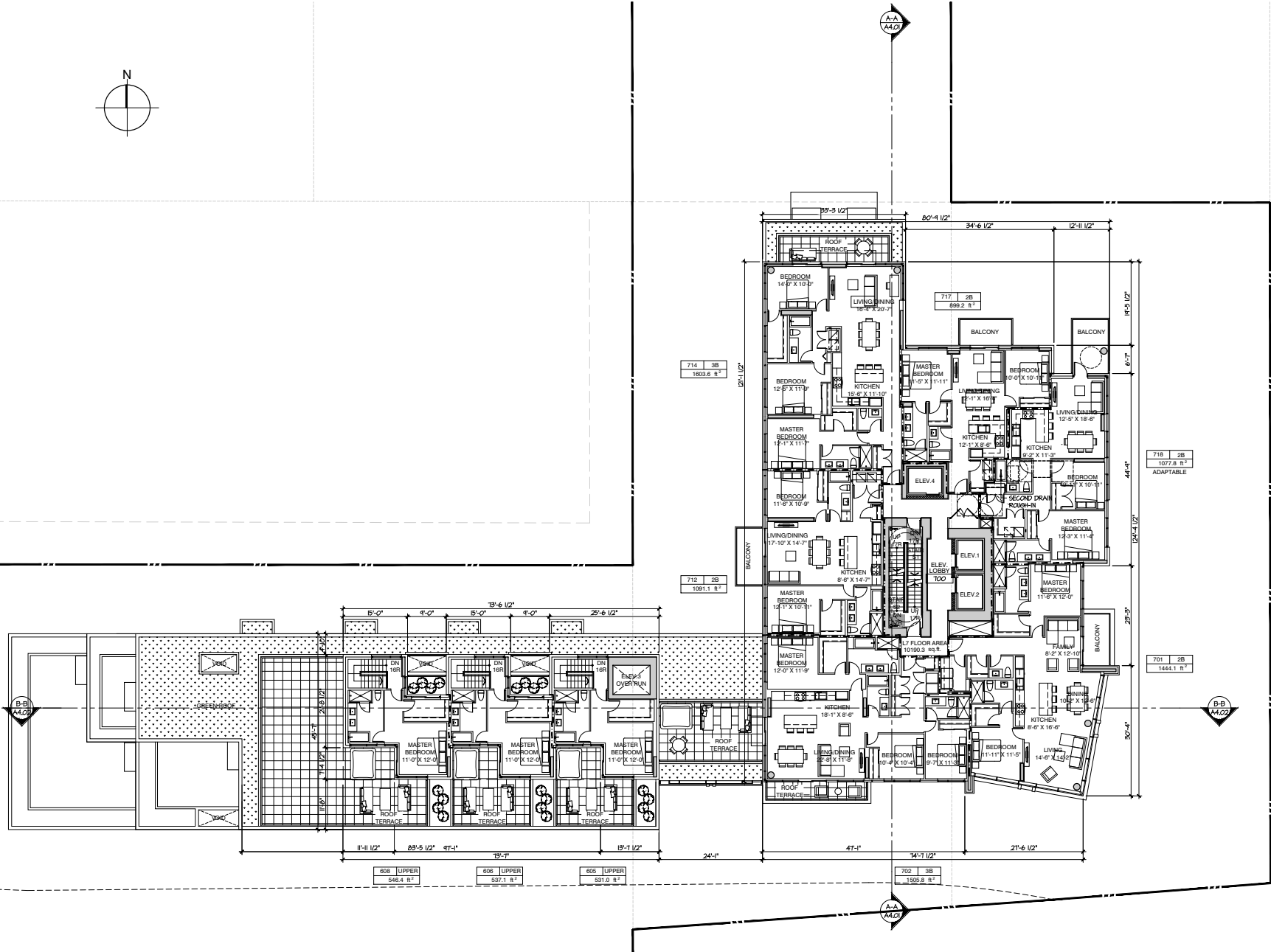
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**PROJECT**  
**EXECUTIVE PARC**  
**MARINE+TAYLOR RESIDENTIAL**

657 Marine Drive  
 West Vancouver BC

**FLOOR PLAN**  
**LEVEL 7**

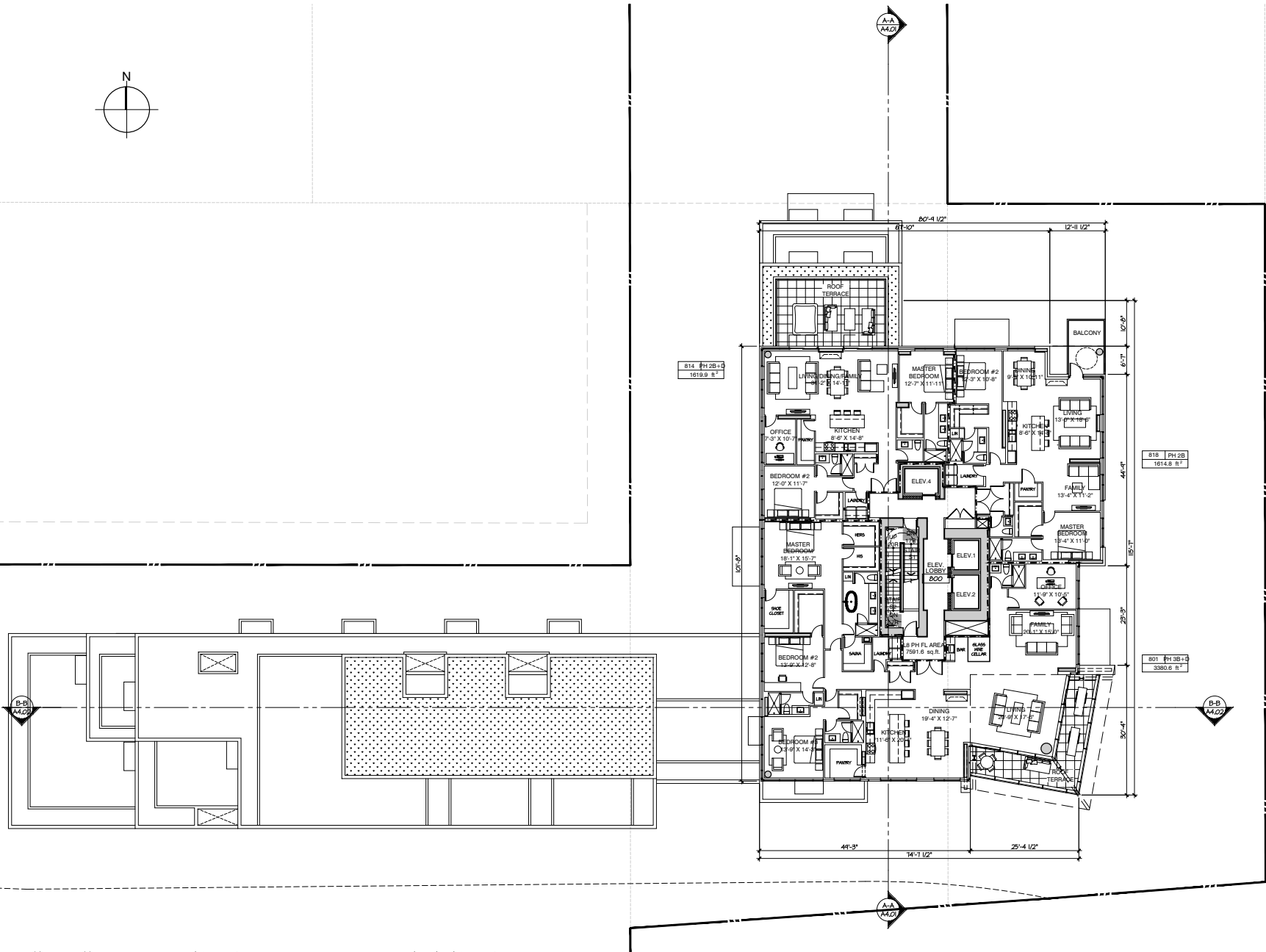
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 DRAWN JL CHECKED NG  
 SCALE 3/32"=1'-0"  
 DATE NOV. 19, 2018

**A2.09**





**dys architecture**  
 260 - 1770 Burrard Street Vancouver BC V6J 3G7  
 tel 604.669.7710 www.dysarchitecture.com

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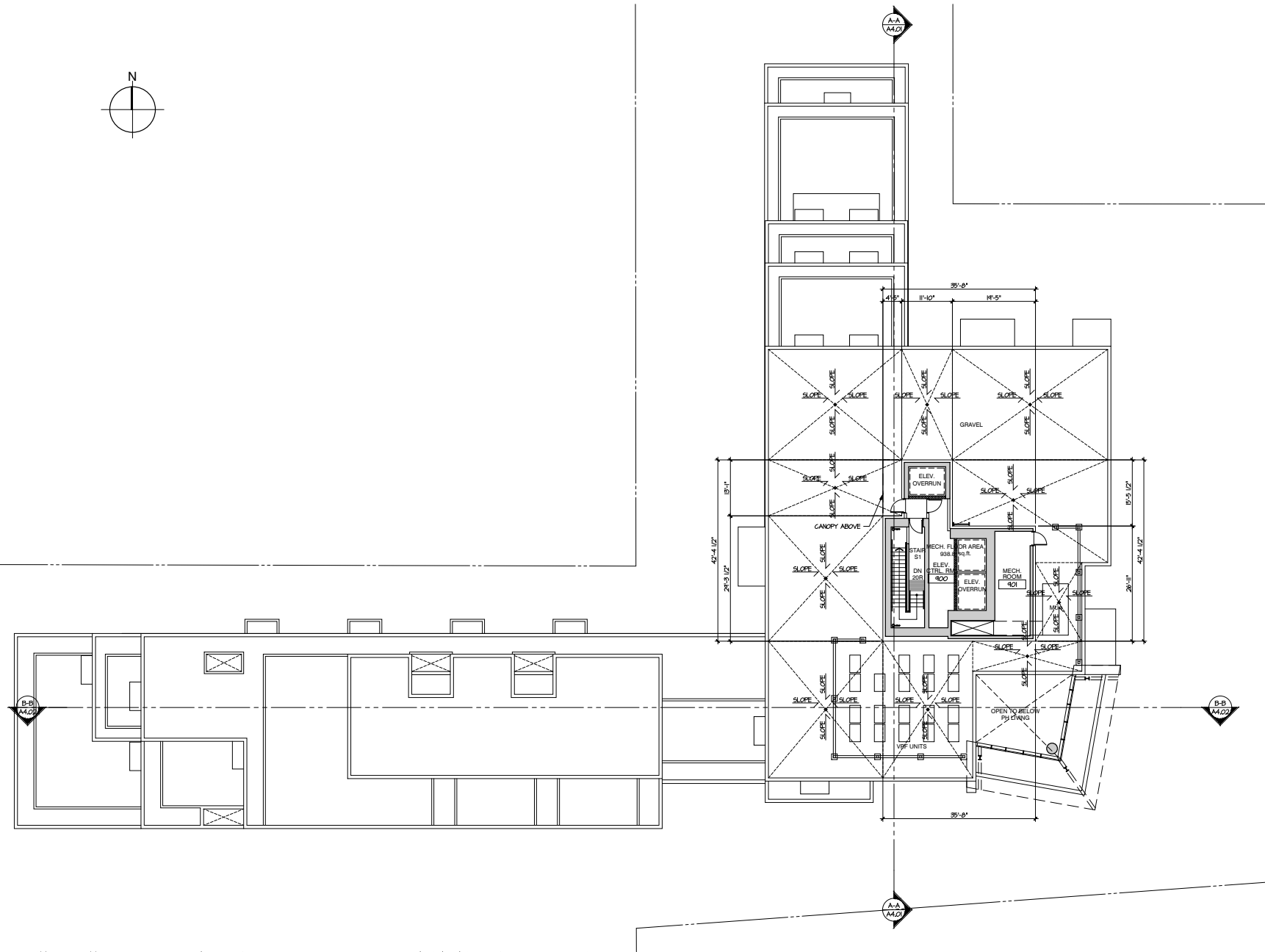
**FLOOR PLAN LEVEL 8 (PH)**

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**A2.10**



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**PROJECT**  
**EXECUTIVE PARC**  
**MARINE + TAYLOR RESIDENTIAL**

657 Marine Drive  
 West Vancouver BC

**ROOF PLAN**  
**MECHANICAL**

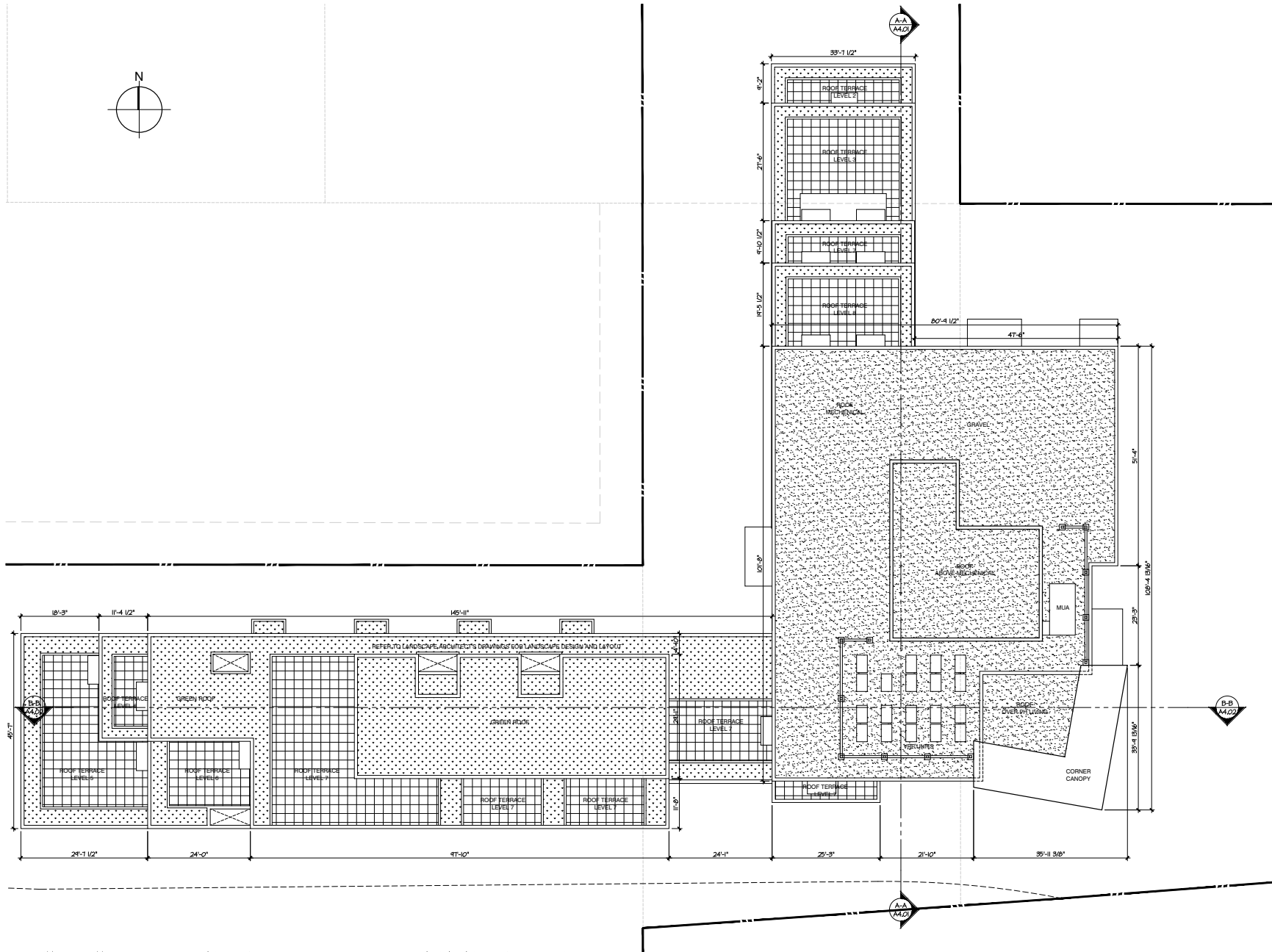
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PROJECT A215294  
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SCALE 3/32" = 1'-0"  
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**A2.11**



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MARINE+TAYLOR RESIDENTIAL**

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**ROOF PLAN**

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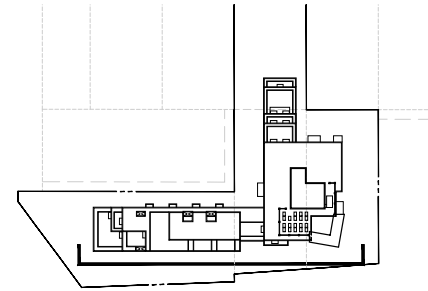
PROJECT A215294  
DRAWN JL CHECKED NC

SCALE 3/32"=1'-0"  
DATE NOV. 19, 2018

**A2.12**



- EXTERIOR MATERIAL FINISH LEGEND
- ① PREFINISHED METAL PANEL - WHITE
  - ② PERFORATED METAL SCREEN
  - ③ PRE-FINISHED METAL PANEL - GREY
  - ④ HOOD GRAIN SOFFIT
  - ⑤ HOOD-LIKE PRIVACY SCREEN
  - ⑥ ALUMINUM WINDOW FRAME - CHARCOAL
  - ⑦ ALUMINUM WINDOW FRAME - GREY
  - ⑧ SPANDREL PANEL - CHARCOAL
  - ⑨ SPANDREL PANEL - HOOD GRAIN
  - ⑩ SPANDREL GLASS - GREY
  - ⑪ PAINTED STEEL STRUCTURE - CHARCOAL
  - ⑫ BRICK - EARTH TONE BROWN
  - ⑬ PRE-FINISHED ALUMINUM & CLEAR GLASS GUARDRAIL
  - ⑭ METAL CLADDING - HOOD TONE VARIOUS COLOURS
  - ⑮ GULLAM CANOPY STRUCTURE WITH GLASS
  - ⑯ HEAVY TIMBER (APPROX. 8"x12")



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**PROJECT**  
**EXECUTIVE PARC**  
**MARINE+TAYLOR RESIDENTIAL**  
 657 Marine Drive  
 West Vancouver BC  
**ELEVATION SOUTH**  
**(MARINE)**

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**A3.01**